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German Magazine for Z Gauge



High summer on the field railway

Fish Belly Bridge Plettenberg
A new advertising locomotive is created

Introduction

Dear Readers,

outdoors the sun is shining, the temperatures are approaching 30° C again - right, it's finally summer. And it is well known that exactly this time of year should be used for leisure time. But is that also true for model railroading?

The model railroad in the cool basement would be an option in hot weather, but is handicrafts fun at all and do people really like to think of models and landscaping during their holidays?

We are absolutely sure of this; so sure that we are referring thematically in this edition especially to the summer.

We build a bridge between summer and model railway, so to speak, and on this occasion we take a look at Märklin's new fish-belly bridge. It's a great kit that doesn't even stop us from sunbathing that long and it's really fun.

Our diorama series "Four Seasons" has been waiting to be continued since its beginning in 2009. Finally it is time, it continues with the... Right guess! We have planned the summer.

Why it took so long and which details can be worked out, if we have the real summer in all its colours and special features "at hand", we would like to reveal today and encourage you to visit the hobby corner yourself.

We witnessed full of excitement how a newly painted locomotive of the 101 series becomes an advertising locomotive on a scale of 1:1. Unbelievable precision, but also improvisation and changes from the original plan during the foiling, all this is a sweat-inducing and very time-consuming work. Only we have pictures and our own impressions, which will certainly give a completely new face to the appreciation of such a work.

But summer time is also time for excursions. So why not visit a museum railway or even a mine railway? In our literature tips we can give you some suggestions. We particularly recommend a visit to the DB Museum in Nuremberg in the near future. It is certainly worth a visit all year round, but the special exhibition "Beer, Railway and Bananas" is only open until 31 October 2019.

Also the manufacturers do not take a summer break, we can announce a few news today again. We especially liked the fire brigades "Kallental" and "Dörpede". The matching fire engine Magirus-Deutz LF can actually be seen on our summer diorama.

Thus the circle closes, the sun shines and we model railroaders enjoy it. Whether you read **Trainini®** on the beach or on the porch or even put the suggestions and tips into action - we wish you lots of fun!

Sin-Z-erely,

Joachim Ritter



Joachim Ritter
Editor

Editorial

Introduction 2

Model

With a curve over the Lennesand..... 4

Prototype

Well dressed14

Design

A sequel with pitfalls.....26

Technology

Currently no items

Literature (not translated - only in German)

Forgotten mine train.....41

To go on holiday to instead of by rail43

News

Beer, bananas and more46

Zetties and Trainini in Dialogue.....54

Imprint.....60

We thank Modellbau Laffont for their support in the run-up, DB Regio for the film permission as well as the Eisenbahnstiftung and Klaus Eckert for photos.

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Cover photo:

The dry and hot summer makes it necessary for the Dörpede volunteer fire brigade to provide fire guards at the forest edges of the western Sauerland. A field train of the Rhene-Diemeltalbahn crosses the journey of the fire engine.

New Fish Belly Bridge from Märklin With a curve over the Lennesand

The penultimate turn of the century marked the beginning of the age of steel truss bridges. One of its variants was the Fischbauchbrücke (fish belly bridge with the arch pointing downwards). Märklin's new product, based on such a model, already radiated some local colour for us and demanded a test construction including for its appropriate use. Shortly before delivery we may now share our impressions with you.



601 017-7 is on its way on August 12, 1984 as special train Et 28190 on the Lenne bridge near Plettenberg-Ohle. The listed bridge over the river and the floodplains, which is Märklin's model for the kit to be discussed, was part of an arc of more than 180°, which was described by the railway line Plettenberg – Herscheid (closed in 1969) in the branch off from the Ruhr-Sieg line. Photo: Wolfgang Bügel, Eisenbahnstiftung

Soon the time has come: Märklin's Fish Belly Bridge (Art. No. 89758) will be delivered at the end of this month. Currently, it is in production for delivery to Göppingen. We were allowed to take a look at the kit in advance and test it.

We were once again enthusiastic about the precision fit of the hardboard architecture kit from Modellbau Laffont for Märklin. Anyone who has familiarized themselves with the technology and assembly of such kits would not want to do without these kits on their system.

In contrast to the polystyrene construction kits that have been available for decades and are no longer supplemented by new products from many suppliers, viewers cannot get enough of the museum-quality architectural cardboard kits.

This is due to the matt surfaces, which appear realistic and do not necessarily require colour treatment, and also to the fact that their production method permits smaller quantities. This leads to one-off and smaller runs, which are constantly being updated with follow-up innovations. So it is no question for us that model railway systems gain individuality and new expressions with this modern technology.

And so we approached the new Fish Belly Bridge from the spring novelties 2019 with expectation, joy and excitement. Its prototype comes from Plettenberg-Ohle in the Sauerland region and is less than an hour's drive from the editorial office. From 1914 to 1969, the Plettenberg - Herscheid route crossed the Lenne River on a total of eight steel truss segments at low heights and with large arches.

In the model, the bridge is of course straight so as not to restrict the buyer from the outset. Its pillars are also higher than those of the original, but here there are optional adaptations for the most fastidious modeller.

In contrast to many models, the supports of bridges in the course of the arch itself are not curved, but straight. The arch is created by stringing together several segments in the curve.



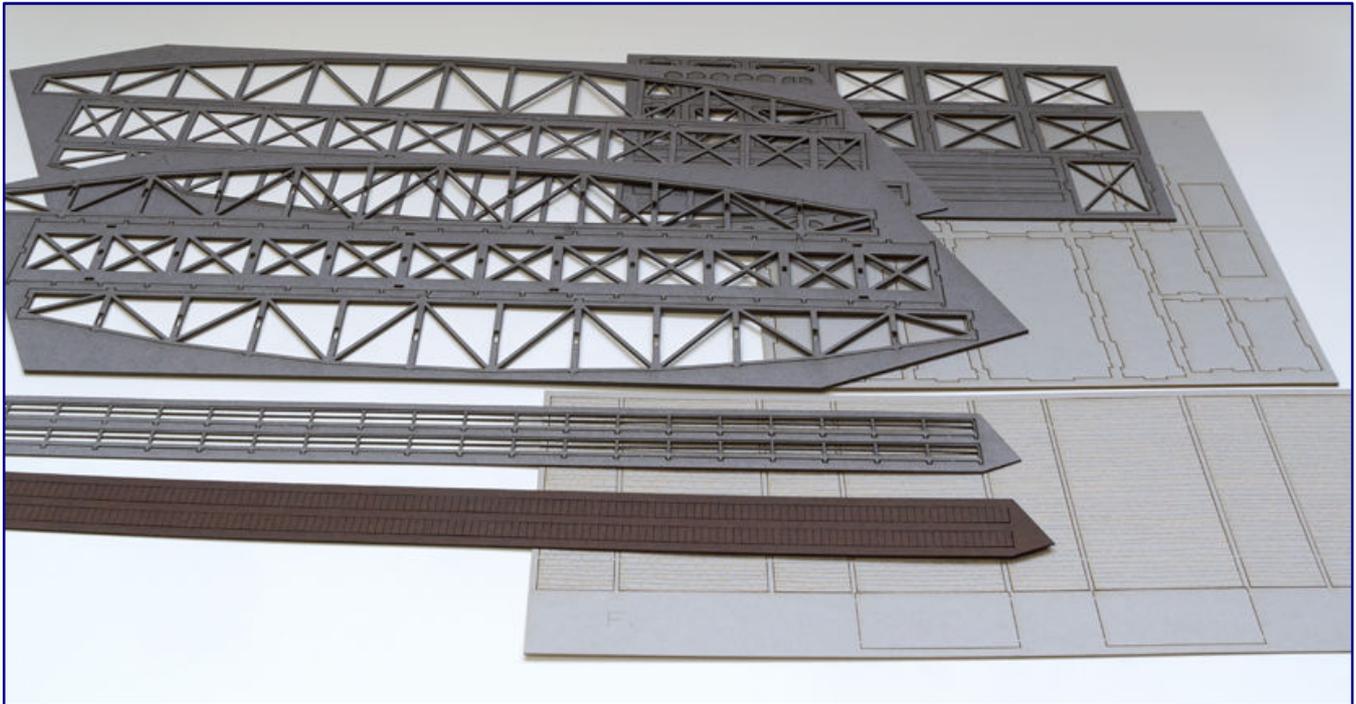
When 601 017-7 and 601 004-5 crossed the Lenne on October 7, 1984, it was easy to see how a steel truss bridge was built into the curve. The special train Et 28269 is on its way back from Hüinghausen to Plettenberg, a train station that Faller has exemplarily converted into a model and that fits the theme. Photo: Wolfgang Bügel, Eisenbahnstiftung

Either way, we would like to leave behind the advice not to purchase the beautiful bridge as an individual kit. It is exemplary and impressive, especially because of the stringing together of several kits.

There should be at least two, three is better. If you have enough space, you can of course build the original model exactly with eight bridge sections. It becomes an eye-catcher in any case when a short branch line train can cross the bridge in its full length and find enough length for the entire train on it.

Construction begins

With these preliminary considerations, we get straight to work. A brief review of the instructions to familiarise yourself with the kit is just as much a part of the preparation as the completeness check of the kit parts. 63 parts from seven sheets in three basic colours (light and medium grey, brown) need to be assembled.



Seven arches with a total of 63 parts await professional assembly. Since the degree of difficulty remains manageable, the Fish Belly Bridge (Art. No. 89758) is also suitable for beginners. A lot of fun is guaranteed.

To ensure that this works well and cleanly, we use the multi-purpose glue “Faller Expert Laser” for all fine and tiny gluing areas and the express wood glue BindanRS for all surfaces.

We prepare a fresh, sharp craft knife as well as tweezers. We classify the degree of difficulty of the kit as manageable.

So, even beginners can gain important experience here without having to fear a substantial financial loss.

Peace and patience during assembly should be a matter of course, because that is simply part of model building! Two to three hours be planned for the complete assembly.



Only a few tools and glues are required for assembly. What is still missing is a toothpick that serves well as a positioning aid.

The procedures correspond precisely with the instructions: The working direction is always from the inside to the outside, starting with the piers, the cores of which are the first in line. Also with the fish belly girder, it is the massive parts of the bridge support and outer arches with cross struts that are built first.



We build the bridge in the order and following to the instructions given in the enclosed instructions. We began with step A, which described the construction of the pillar body.

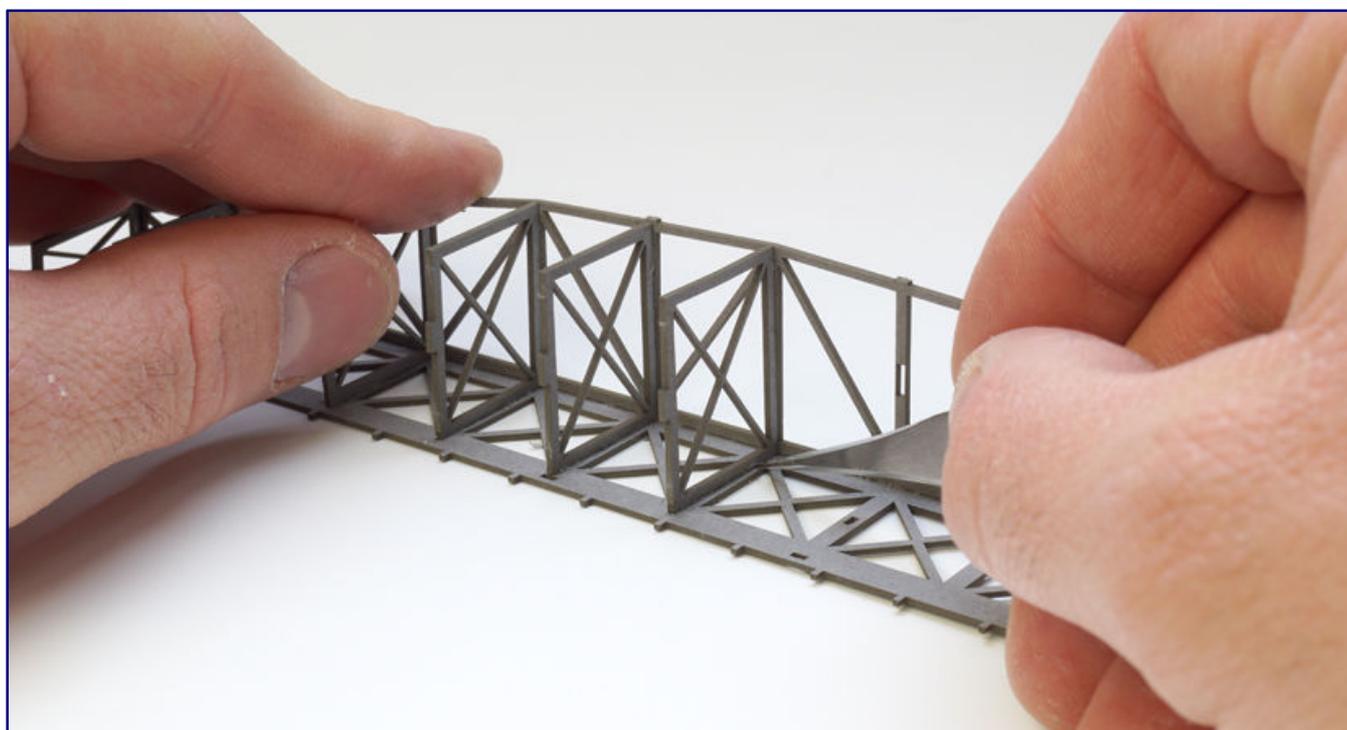
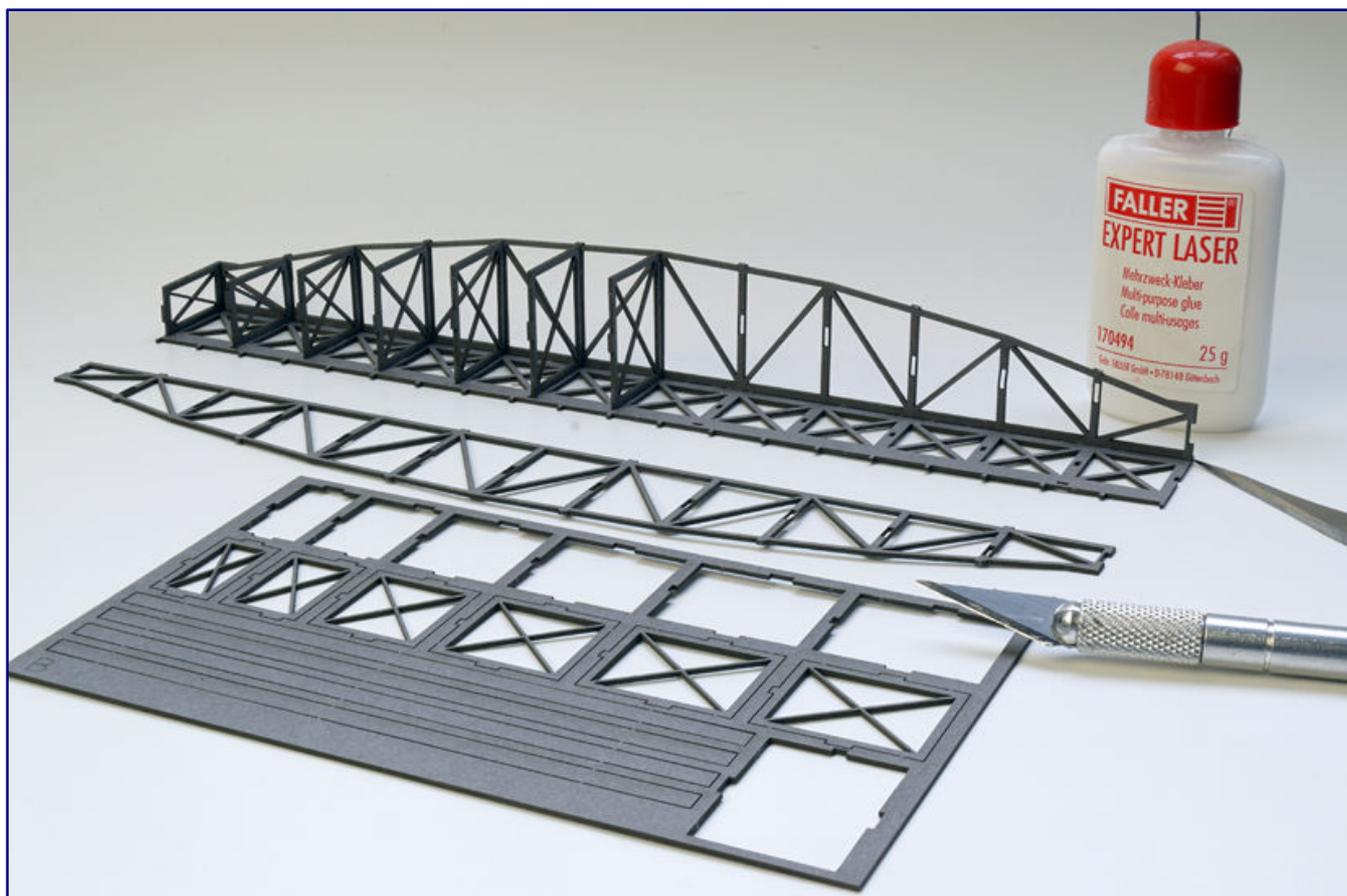
This step proves to be the most demanding. Here, no components may be interchanged, therefore we refer to a helpful Laffont principle: Identical elements always have the same part numbers, even if they are present several times in the kit.

If the designations differ from each other, the associated parts are also different, even if the differences are not immediately obvious. It is, therefore, mandatory to take a close look and identify the parts and their locations.

This is important in the specific case for the individual steel truss elements below the bridge deck. They are to be glued one after the other in accordance with the prescribed arrangement transverse to the direction of travel as soon as one of the two fish bellies is inserted and glued. The second is placed at the end of this section.

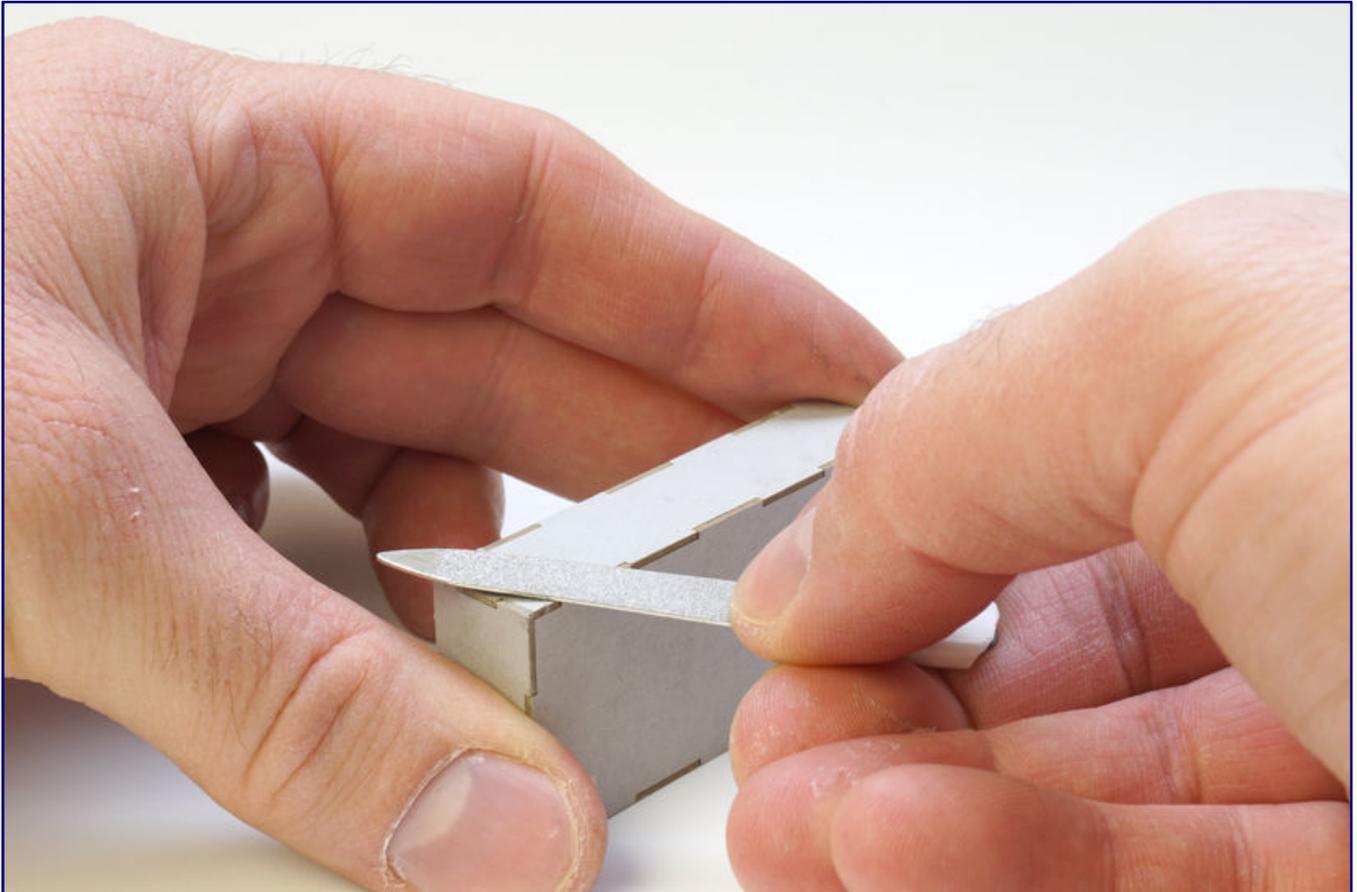
Clean working is just as important. If some glue swells out at the edges, it is carefully removed with a toothpick or a pointed tool, which was made from a defective pair of tweezers, as soon as the glue overflow appears. We even grind the edges of the pillar cores a little to ensure an absolutely flat surface.

continues on page 9



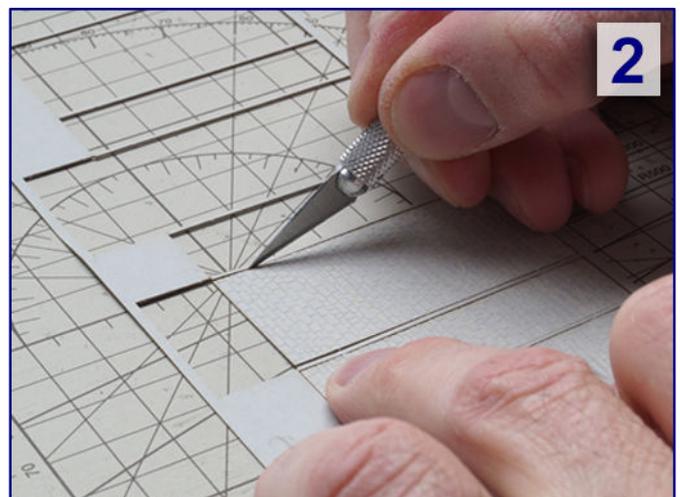
After gluing the first fish belly girder under the roadway, the cross beams follow. It is important that they are not inserted in a reversed position (photo above). If some glue emerges from gaps, we wait until it has tightened a little and then remove it with a pointed gluing tool from a broken pair of tweezers (photo below).

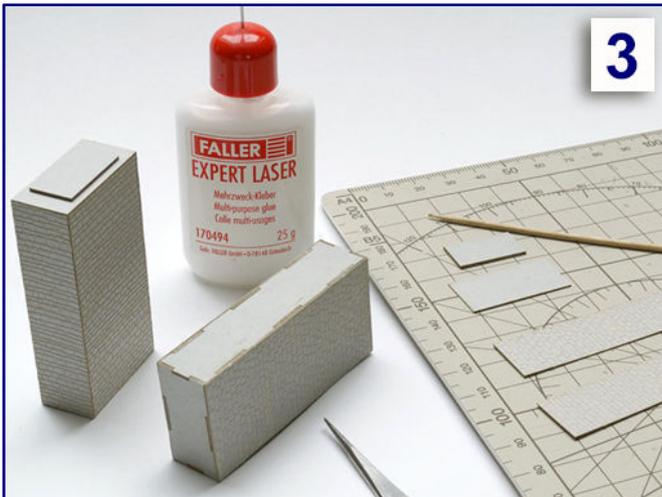
We continue with the masonry facings and abutments on the upper side. First the cover cardboard of the long sides is attached, and only then do the two front sides follow. The pier construction is completed with the thin cover plate on the upper side.



Before we apply the masonry veneers, we carefully grind off (smooth) all the teeth of the already glued parts. In this manner we prevent unevenness of material or glued areas, which could later affect appearance.

A part made of stronger cardboard is glued to it, which holds the bridge head in position as a bearing. A centring aid is not marked, so it makes sense to do a fit test without glue to familiarize you with the correct distances of all four sides before final positioning.



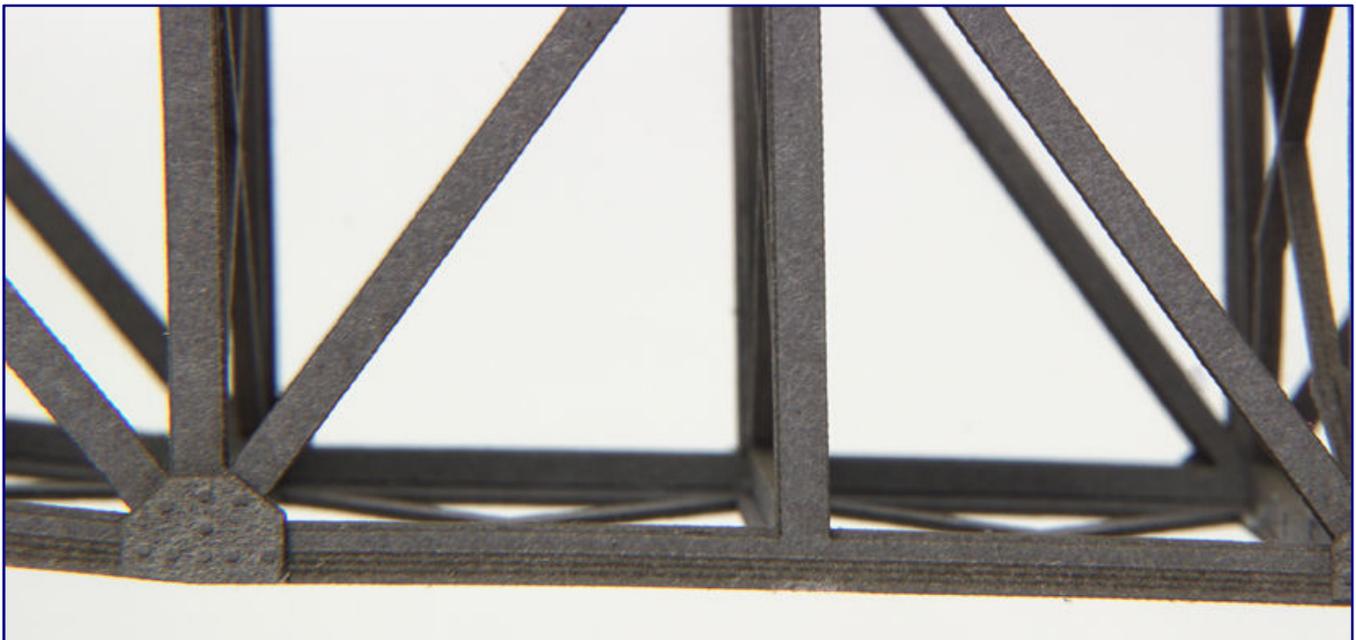


Covering the bridge abutments in four steps (page 9 bottom / page 10 top):

Do not apply glue to the surface of the cover carton. Small glue dots in the corners, at the edges and on parts of the surfaces are sufficient (Fig. 1). The masonry facings need to be carefully cut out of their thin arch (Fig. 2). The longitudinal sides are glued first and then the narrower ends (Fig. 3). At the end the base plates follow on the upper side. The supports for the bridgehead are first subjected to a dry test for correct alignment (Fig. 4).

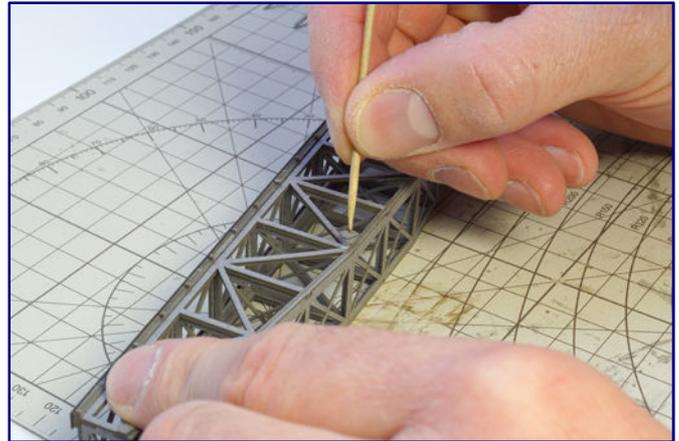
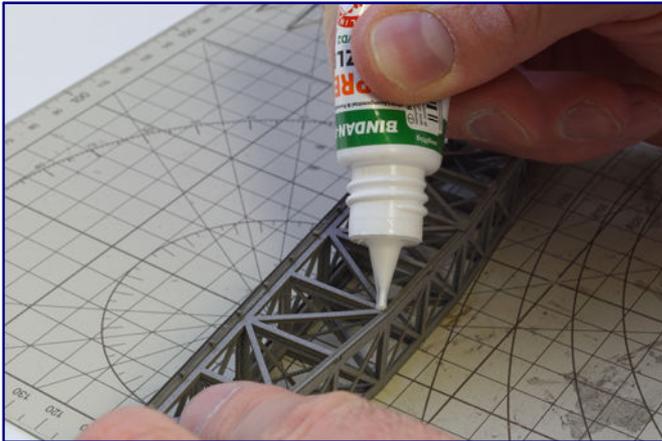
At the bridge itself, the “outer skin” of the lower portion in the curve now awaits. The gusset plates, which clearly mark this bridge as a riveted work from the turn of the century, are then glued in place.

It is important to pay close attention to the illustrations of the individual shapes and their part numbers so that each part finds its correct place and position. Thanks to detailed instructions and clear marking, this is not a problem as long as we remain attentive and do not act too hastily.



Besides clean cuts and engravings, the gusset plates, which must not be swapped during application, also play a major role in the overall appearance.

Of course, all components are separated with a hobby knife mentioned at the beginning and with practiced cuts. Precision and skilful cuts are important so that there are no tear outs.



Gluing on the gusset plates is a fine job: a tiny drop of glue is all that is needed, which can be dispensed precisely to size and point thanks to the dosage cannula (left photo). The toothpick helps to position the tiny cardboard part (right photo).

The tweezers prove to be helpful, because the small parts, in particular, can hardly be gripped and positioned precisely any other way.

A toothpick tip helps to set fine adhesive dots without staining, unless the Faller adhesive is used in such places. The toothpick is also indispensable as a positioning aid.

The construction of the bridge progresses quickly, because there are only five working steps to the finished model. The fact that they have been divided up in a meaningful way, and presented graphically makes it easier for us.

Finally, running boards and railings are on the agenda, after which the first bridge is already waiting for installation on the system. Repeating this work several times is not a problem and does not lead to boredom. The impact of the extended Fish Belly Bridge compensates for everything in the end, anyway.



After the railing has been put in place and the running boards have been attached, the Fish Belly Bridge is ready. It looks good, but its impact is increased considerably when several elements are added together.



The bridge looks so beautiful when it is set in scene in the light of the setting summer sun: the longer, the better. And with several construction kits, it can also be used to create outstanding paradise stretches.

The new bridge can be used from epoch I to the present day and offers a wide range of options. In addition to the impressions described above, this is another reason to nominate this model for the best new releases of 2019 in the category accessories.

.....
: **The website of the supplier:**
: <https://www.maerklin.de>
:
: **The website of the manufacturer:**
: <https://www.modellbau-laffont.de>
:.....

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Märklin's new advertising locomotive 101 064-4

Well dressed

Do you remember “150 Years of Märklin”? That was in 2009 and an elegant black class 120 ellok promoted that anniversary. Since the company had to file for bankruptcy a few days after the International Toy Fair, it also became known as the “Trauerlok” (funeral locomotive). That was ten years ago, Märklin is celebrating its next anniversary and is doing so much more colourfully with a new locomotive.

It's June 29, 2019, 6:00 in the morning. In the Dortmund plant of DB-Regio is now at the time of shift change. But it is no ordinary Saturday morning. In the halls, the workers who worked the early shift see a long-distance locomotive: 101 064-4 is standing here, washed and thoroughly cleaned and freshly painted. It wasn't until April, that it was inspected and repainted in Dessau.

Therefore it was chosen to put on a “fresh robe” and to be allowed to advertise Märklin's 160th company anniversary. So the youngest of all advertising locomotives currently in the Deutsche Bahn AG fleet is before us. It thus continues a tradition that began more than twenty years ago with a Christmas locomotive.



In the best summer weather, 101 064-4 with its advertising design “Märklin verbindet Generationen” (Märklin connects generations) rolls out of the engine shed early in the morning of 30 June 2019. Before she returns to regular service, the Märklin documentary will photograph her and record every detail for posterity and compare it with colour cards. Photo: Klaus Eckert

If the machine could think, it would probably be most curious about how it would look after the transformation. But at the moment only the two experts who brought plans and foils for their design, and are diligently arranging and studying them know this.



The plans for the two different sides of the new Märklin locomotive are available throughout the entire gluing process. They help if individual slides cannot be assigned or aligned at the first attempt. Detail on the edge: No company number has yet been noted on the graphic representations.

But soon the work begins. The first rolls are pulled out and sighted, measured and tested. A work that has been followed with anticipation takes its turn, because we witness how an ordinary red locomotive becomes a moving work of art step by step.



The first films are sifted and rolled out. The gluing will start soon.

But until then, there will be a few more hours in the country. At 14:00 hrs the DB Regio shift ends, the later appearance can already be guessed.

But 101 064-4 is by no means finished, the process of applying the film continues into the evening hours.

However, the end of the work is quite precisely specified: On Sunday, at 9:00 a.m., a Märklin Documentation employee will arrive.

His task will be to photograph the locomotive from all sides, record details and compare the colours used with the colour cards and fans he has brought with him.



Work on the side walls is progressing rapidly. The first long side with the metal construction kit motif is already finished, but it gets tricky when the sloping front and roof areas are reached.

This is the only way to ensure that everything is recorded for posterity and that a coherent model can be produced at any time. Even if no models have been announced yet, the big locomotive will certainly not remain alone for long. Why else would Märklin invest in a moving advertising space?

But this Saturday there is still nothing to see or suspect from all this. While the maintenance staff are busy maintaining or repairing locomotives and railcars, work on 101 064-4 is progressing quietly. Their metamorphosis is accompanied by the camera of our editor-in-chief and the camera of Märklin-TV.

Interested rail fans and Märklin model railroaders can also participate in this process. If they can enjoy the balanced picture of the finished advertising locomotive, this will certainly also be good for the sales of the models, which Märklin will soon release.

But we are starting to dream. Back on the ground, we have to follow their development closely and capture the most important steps. We are happy to receive any explanations on the individual work that we repeatedly receive.

What looks so simple at first glance is a long and demanding job that requires a lot of skill. Our two experts, whom we are allowed to observe, can be seen in their routine.

Nevertheless, there are situations in which things get tricky and not everything wants to fit properly right away. These situations require prudent thinking and an adjustment of the plans in order to track down the devil in the detail.

continues on page 18





Picture above and page 17:

As soon as the first foils are attached to the locomotive after fixed orientation points, all further foils will be applied step by step. They are aligned and only temporarily glued to individual points because critical points such as the gutters on the driver's cab side window still require reworking (top photo). Further to the right, the mounting of the access rail has already been cut out (photo page 17) and the foil is now spread out with the plastic spatula.



Here, too, it is necessary to cut the base free for one of the access rails.

In addition, the constantly rising temperatures make it increasingly difficult. When the thermometer has jumped well over the 30 degree mark outside, the heat is also noticeable in the throat.

Further challenges are posed by the locomotive itself: The areas to be covered reach almost four metres high up to the fasciae of the roof cladding.

Ladders and mobile working platforms with railings are required to reach them all. Despite this, it is sometimes not possible without stretching by the participants.

In the beginning, everything goes quite fast. From bottom to top, the first side surface is covered with foils in horizontal rows. We call it the metal construction kit side. The opposite side, which follows next, are more difficult to photograph. It shows historical motifs from the Märklin H0 range including its predecessor in the form of the E 03 series.

continues on Page 20

Auftrag: MAERKLIN_LOK_RECHTS_SEITE_TEIL_1B.PRINTING)



Cutting out the surfaces along the gutters and windows is more difficult than it looks (photo above). The problem areas are the slanted parts of the locomotive, which also have an arc to the front. The foils here have to be laid flat and aligned exactly, so combined forces are required (photo below).

Photo page 19:

All foil parts are clearly labelled and numbered. So that no seams appear later, they are printed overlapping, as can be seen here on the V of the diesel locomotive.

The smooth walls make it easy to apply and align the foils. If everything fits, the protective foils on the adhesive surfaces are removed and the motif prints are finally connected to the outer sheet metal of the



Accompanied by cameraman Andreas Stirl for episode 99 of Märklin-TV, the work now continues on the model railway page. The fronts prove to be the biggest time wasters, because nowhere else are so many fitting and cutting jobs necessary, as here.

locomotive. “Dürr cookies” (nickname of the actual DB logo) and company number disappear under the advertising. They are later replaced by separate stickers where necessary.

The process of film gluing that we are allowed to follow here differs from what we have seen and learned a few years before in the context of motor vehicle gluing. There, the films were often applied to a layer of soapy water in order to allow them to “float” and still move and align them.

This is then comparable with the application of wet decals in model making. However, the large areas of a locomotive obviously require a different procedure. Here, the protective foils are initially only removed at selected corners in order to allow them to adhere to the vehicle body. They are not completely removed until the drawing file is completely and correctly aligned.

Again, the smoothing to the edges with plastic spatulas is identical in order to obtain a seamless and bubble-free film surface. The image shown on the locomotive will end up with many small single images with little overlap. On white edges, text markings facilitate the assignment of all components to the planned overall image.

Surfaces such as windows are simply glued over in this phase and cut out again shortly afterwards along the seals. The cuts must be made immediately on the supports for the driver's cab access rails, as they protrude from the wall and would otherwise cause distortion.

Again and again small bubbles occur which cannot be completely removed despite all efforts and care. They are then punctured with a needle so that the air can escape - problem solved.

Slowly, our two experienced experts work their way up to find out that the locomotive is not equally accessible from all sides. The large, mobile working platform cannot be positioned to allow access to all points on the locomotive.

Since 101 064-4 stands under a hall section without contact wire, otherwise such work would not be possible due to electrical arcs, it cannot be moved by a locomotive driver. So now the men from DB-Regio have to come in.



Four strong men are enough to overcome the rolling resistance and move 84 tons of service weight on the rail. Our protagonist 101 064-4 has to be moved twice, because otherwise the roof areas on all four sides cannot be reached. After all, the mobile working platform still needs to have room.

One of them releases the spring-loaded brake in the engine room and four others gather at the buffer beam to manually move the 80-tonne colossus. This works for freight wagons and passenger coaches, but also for a locomotive? Yes, it works, because the machine slowly starts to move and is then braked again. The gluing continues immediately afterwards.

The corners, slopes and especially the curves become exciting later on. If a two-dimensional film is laid over this structure, reworking cannot be avoided. In order to avoid distortions in the advertising motif, the designers of the motifs do not create pictorial representations in these problem areas.

At the critical ends of the new advertising locomotive, the beige base of the motif with the striking lettering “Märklin verbindet Generationen” (Märklin connects generations) is simply screened out to the basic tone that deviates only slightly from traffic red.

This smooth transition is well balanced and appealing, but also poses challenges for the experts, because the many points must be precisely assigned and aligned according to size and distance. In addition, especially in the upper area of the driver's cab side, where both bevels and curves meet, a lot of reworking would be necessary.



The front sides take shape:

First, the foils of the sloping side surfaces overlap parts of the front. They still have to be cut off at the edges as soon as all gaps have been closed (Fig. 1). The working direction here is from top to bottom. You start with the Märklin lettering (Fig. 2). The area under the front windows makes a lot of work, because cuts for windshield washer nozzles, windscreen wipers, UIC socket and handles are required in this area (Fig. 3). Important operating instructions, such as those on the apron here (Fig. 4), must not be omitted when making the cuts. The final front can be seen on page 24.

It takes a few hours until everything is fitted, and the foils are cut with high precision at the problem areas in such a way that all parts fit together seamlessly. A further challenge is the two front sides, on which the DB emblems and Märklin lettering are finally emblazoned. The absence of the contrast bars next to the logo seems strange. The thin white frame, which normally sets it apart from the red background, has also been omitted.

continues on page 25

märklin

DB

101 064-4



The entire surfaces above the buffer planks are also covered with foils on the fronts. This may come as a surprise, as they were and remain red. But the client certainly doesn't want to risk any colour deviations from the basic colour. It is also possible, however, that water and dirt could infiltrate the edges pointing forward when driving fast in the rain.

Nowhere else on the locomotive are there so many places to cut in and out as in this area: handles, windscreen wiper attachments, washing water nozzles, sockets, driver's cab front windows and lanterns are to be exposed cleanly in order to maintain all functions.



The locomotive is completely foiled and the result can be seen. The design with historical catalogue motifs comes from Märklin's in-house advertising department.

Finally, fine work is carried out on the lower sloped area. There, a panel section extends over the end of the frame on all four sides. They are normally painted traffic red and are therefore also included in the advertising design. The tricky thing is that important operating instructions are also attached with stickers here.

They must therefore also be cut free, which must be done extremely carefully so that it later appears as if these instructions are on the foils of the advertising design. Almost twelve hours have already passed, and we understand the high demands of foiling such a large object as the 101 series.

Even removing film is not as easy as we thought it would be. It is crucial to know from which manufacturer the films used are obtained; here it is 3M, and which physical properties they possess. Their age, i.e. the time elapsed since they were applied, is also decisive for the type of procedure and the time to be estimated.

But we are far from there! At least one year, we hope, will see 101 064-4 in their anniversary design. One of the most beautiful advertising spaces, which ever rolled through Germany, first wants to be produced as a model in different scales, before we want to say goodbye to it again.

The pages of the client:

<https://www.maerklin.de>

Märklin-TV (episode 99) to the creation of this locomotive:

<https://www.youtube.com/watch?v=fxaYdKa19GY&feature=youtu.be>

The four seasons of Z gauge (part 2)

A sequel with pitfalls

In the November 2009 issue, almost exactly ten years ago, we announced a diorama series “Four Seasons”. It all started with autumn, but the rest has been a long time coming. However, the continuation within the editorial staff was never forgotten, rather new products offered better possibilities, while other topics required a postponement of the series. For last year's EM diorama competition, Part 2 was finally completed, we present it here today, as well.

Summer 2018 was one of the longest and hottest summers ever recorded by meteorologists in Germany. But while some of them enjoyed more than ten weeks of open-air swimming pool weather with temperatures regularly reached via 30°C, others sweated in the brooding heat.

Nature has also given little pleasure this summer of the century: The potato harvest was modest, and many other crops also suffered from the drought. Pests have increased enormously, as we unfortunately had to find out this year. The groundwater level has not fully recovered to this day.



The hot and dry summer of 2018 brought the idea for the second diorama of the Four Seasons series. The theme was the continuing high risk of forest fires as a result of high temperatures and lack of precipitation.

Another permanent topic was the high and ever increasing risk of forest fires. Many people are certainly aware that not only carelessly discarded cigarette butts can cause widespread fires with great damage

to people and nature. Even a carelessly discarded bottle can act as a magnifying glass and trigger a forest fire.

Therefore we had the idea to deal with this topic in connection with the model railway and to focus on our sensitive environment. The theme of the second part of our Four Seasons series should therefore be called "Fireguard" and show a dry summer landscape at the edge of the forest.

The contrast of a red fire engine should draw attention to itself and clarify the imminent danger. The fire engine should be alone and make it clear that it is not on its way to an extinguishing mission. Rather, it is about another of the four tasks of the fire brigades: Protecting (the population from danger).

So far, the question of how the diorama could slip into this year's focus remains open? This is also answered quickly: Originally, a spring diorama was planned for the second part as a continuation of the seasonal series.

However, its construction was always delayed, because this too was to have unmistakable features that would distinguish it from early summer without a doubt and at first glance. The idea was to create a flowering horse chestnut that would be beautiful to look at and that would have all the characteristics mentioned above. A timely publication at the time of the year seemed to us to be an obligation, so that our readers could compare this in nature.



It was clear early on that the beautiful Saller light railway would finally be staged at the summer diorama. This is where the O&K diesel locomotive sets out to cross the level crossing on the country road with its short train of box and tipper wagons.

And so the spring became summer, with it should also change the theme. The course of summer 2018 then allowed the planned performances to mature further. But then the timetable fell by chance.

Dietmar Allekotte appeared with his coastal diorama, which he had built for the EM diorama competition, for the photo shoot. In this context he was allowed to inspect works that were in preparation for the magazine.

And so he found out that the summer diorama also follows the competition guidelines. It remained to be seen whether participation would also be admissible in view of the designed area, which was considerably smaller than allowed. As an answer to a question to the editors, a participant number promptly came back.

This made it clear that the planned report would have to take a back seat because the diorama would initially take part in the competition. After all, its design was no longer allowed to become publicly known in order not to influence the voting behaviour. This finally resulted in the new publication date in July 2019.

Choice of motifs shown

The planned landscape had to be clearly and unequivocally classifiable as summer. As a result, the green should not look too fresh and juicy, and even grasses should, at least, partly look dry.

It turned out to be interesting for our research that fly mushrooms, in contrast to many more inconspicuous representatives of this species, are not typical for autumn, but spread their caps in July. The fact that they are mainly to be seen on the ground under spruce trees also determined the desired vegetation.

As the topic of an extremely dry summer came more into focus, the conifers came further towards us, as they made it much easier to depict groups of trees that suggest a forest edge instead of solitary trees.

Another desired theme was to show something different, instead of a normal gauge railway. In general, the street should play a bigger role here anyway, which further limited the area for railway lines on the small diorama. After all, new car models also want to be shown in a suitable environment!

At no time was it a matter of explicitly aligning the diorama with the competition and giving it a central eye-catcher that would catch the eyes of every visitor. Rather, the idea was to create a photo backdrop that would later be used in daily editorial work.

There are therefore several small scenes that speak for themselves and want to be discovered for themselves. They can be captured and highlighted with the camera or the observer takes



A diorama stands or falls with whether it knows how to convey life. A wanderer scene with Trafofuchs figures (photo above) and the very successful fox of C-M-K (photo below), which invites to a search game, were selected for this.

the time (at exhibitions) to “go off” centimetre by centimetre with his eyes, and that's exactly what wouldn't happen in the competition.

The specifications described were best met by a light railway. For years we have been fascinated by the models offered by Saller, which disappeared from the focus of the Zetties for a long time, but still offer many products with great designs.

The ideal, but very freely implemented model was provided by reports by Jörg Erkel (1zu220-Shop), in whose vicinity the Rhene-Diemeltalbahn once operated and also crossed a country road at least once. Since this light railway has long since been dismantled, impressions could only be gathered from the specialist literature.

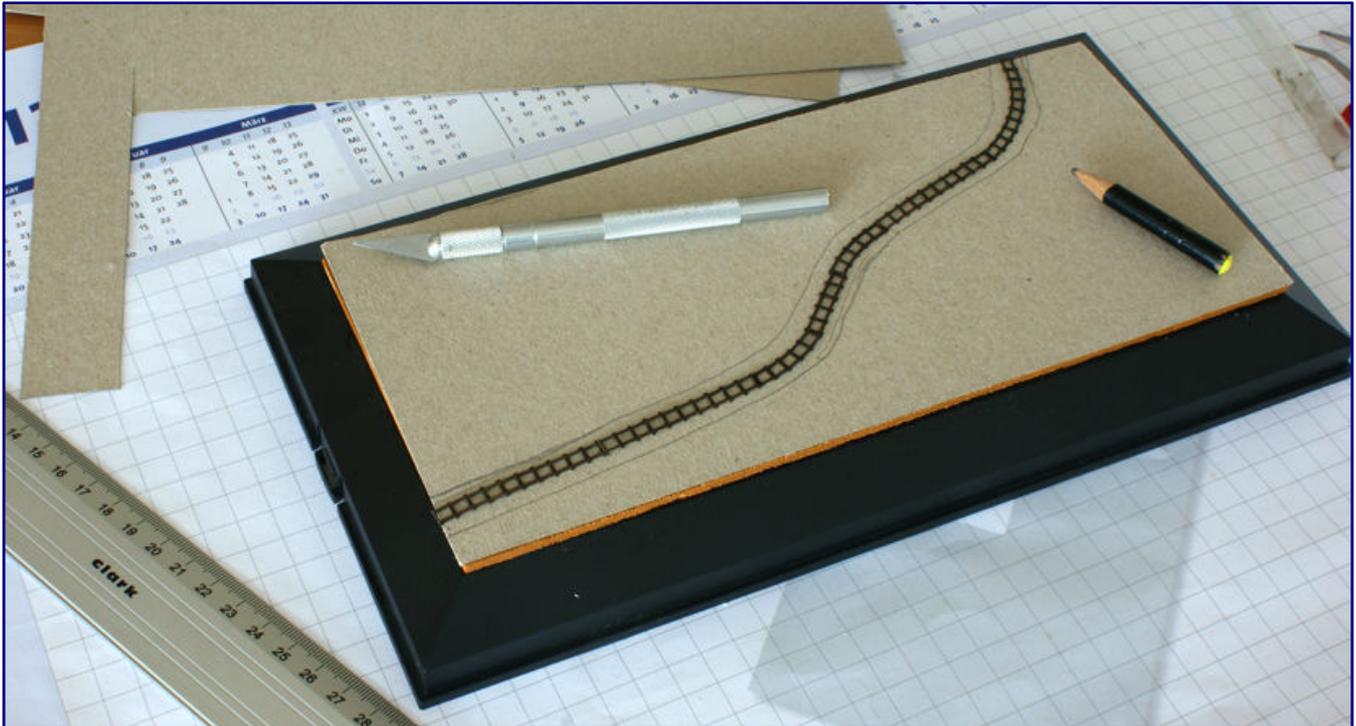
The construction of the summer diorama

The diorama is embedded in a universal collector's showcase made of acrylic glass which we found in the Conrad Electronic program (order no. 211389). Their dimensions of 30 x 15.5 x 11.5 cm had already been determined with the first diorama of the series for all objects of this series. The background of the diorama in it is a plywood board, which was glued to the plastic base with Uhu Max Repair Universal, a powerful power adhesive.



A lacquered plywood board forms the base for the construction of the diorama. It is glued to the plastic base with a special glue from Uhu and fixed there with screw clamps until it is completely dry.

In order to be able to plan the terrain structure better, the following construction of the scenery starts with the country road and the field railway line crossing it. To do this, we first lay a piece of drawing cardboard in the format of the total area shown and draw the planned road and route with a pencil.



On a piece of drawing cardboard, which is the same size as the base plate, we first draw the line of the light railway (photo above), as soon as we are satisfied with its course. Then follows the route of the country road, with which we can cut out both traffic routes (photo below). It is important for a realistic effect that the routes are not laid parallel to the edges.

First of all, we use various Saller track pieces made of white metal to lay out different routes, and assess their impact on the observer. This requires a certain spatial imagination, because the planned landscape structure should be included, as far as possible.

In order to open the lines of sight for the camera and achieve a clear end to the rear, the scenery will rise slightly to the rear. Also the country road should not be completely level so as not to be boring, and will have a slight gradient to the left.

Therefore, the light railway has to lie there in front of the road in order to be able to photograph the trains unobstructed. The level crossing will be in the middle of the diorama, on the right the narrow-gauge railway can disappear to the back in a terrain incision. A harmonious and credible track layout is quickly found under these conditions and is drawn on the cardboard with the later track width.

We defined the road width as 6 m, which fits well into epoch III, when the vehicles were usually narrower than today, which is why even minor roads had a smaller road width. The dimension converted to our scale is therefore 28 mm. With this width we also record their course as soon as it is certain for us.



The street on the left, which rises slightly, is already lined and glued with further cardboard strips. Now it is time to treat the drawing cardboard with a deep base and make it largely insensitive to further work.

After cutting out the road and track, the rest of the cardboard can be glued to the base plate with Uhu Hart. To prevent it from warping from the water of glue or dispersion paint, due to its rather low quality, we treat it with (water-based) deep primer from the DIY store. The gradient of the roadway has already been created and glued with the underlying parts.

As soon as the primer has dried thoroughly, it was time for the rail parts to be glued on. Before this, however, they receive a paint treatment with the spray gun. The parts are not colour treated ex-works, and therefore still look too new.

We imitate the usual rust tone of outdoor iron and steel parts by fogging them several times with “Sepia Shade” from Vallejo (73200). This glaze still allows the basic colour of the metal used to shine through and is therefore particularly realistic.

After drying, we sand the rail heads free again with fine emery cloth, because here the wheels of the locomotive and the wagon ensure that no rust gets stuck on them. After all, our field railway is still in operation and has rail traffic several times a day.



According to the defined viewing axes, Modur foam pieces are stacked and fixed with contact adhesive and glued together (top photo). Then they are roughly cut to size and processed with emery cloth (photo below). And at the level crossing, the road surface is adjusted to the level of the rail heads. Painting with primer is also necessary here.

After drying, we determine the course of the landscape with various pieces of a foam slab (purées). This material is used in the construction industry and is sold in the usual sizes and quantities for model railway purposes. It can be modelled with knives, saws, files or sandpaper just as easily as Styrodur.



The roughly designed landscape and the ramps of the country road are given a smoothing coat of Molto Fine Spatula. After drying, the sand-like wood repair putty of the same brand provides a realistic surface structure.

Instead of electrostatically charged parts, fine crumbs are left behind, which are to be absorbed promptly before they spread throughout the room. An advantage of Modur over the alternative polystyrene material is its resistance to solvents in adhesives.

For example, we do not have to take into consideration the bonding of the individual parts to the base plate or to each other, nor do we have to take into account the subsequent bonding in holes, such as may be necessary for trees.

But we are not there yet. First we have to close the remaining gaps after levelling the structures. Here the Molto-Feinspachtel is used, which our editorial colleague Dirk Kuhlmann also swears by. It also serves well for smoothing the road area in the area of the asphalted level crossing.

We later create the rough landscape structure with the coarser Molto Wood Repair Filler, which sand structure is ideal for our landscape. Now the time has come for the first test installations of the HOS traffic signs and the new crash barriers.

In particular, the position of the St. Andrew's crosses (railroad crossings) needs to be carefully considered so that they stand sufficiently far from the clearance of the light railway, but can still be seen well and in adequate time by road traffic.

Landscaping

With the already completed steps we now reach the phase of greening. It begins with the application of suitable soil colours. The basic colour design is carried out by a glazed application of chocolate brown tinting colour from the DIY store.

It only dampens the inherent colour of the wood repair putty slightly in the direction of natural-looking tones. The matt acrylic paints in dark brown (61189) and light green (61194), on the other hand, create contrasts.



Now tinting paint and two Noch acrylic paints are used to give the surface an earth-like appearance. We paint the country road with Faller's street paint.

We imitate the road surface by applying the darker Faller road colour (180506), which is better suited to the road depicted here, than the light concrete colour, which is more suitable for contemporary high traffic streets.

For the ballast of the Saller models' field railway tracks, the professional ballast limestone N/Z "beige-brown" from Noch (09161) is used. It is processed in the classic floating way.



The Noch ballast is applied with a thin cardboard dosing aid and pushed to the sleepers with a bristle brush. It is glued wet with a mixture of white glue, water and a drop of detergent. But first it is wetted with the spray bottle so that the tiny stones do not float on contact with the ballast adhesive.

For this purpose, it is first spread dry on and next to the tracks with a dosage aid made from a folded piece of a separating sheet and then pushed straight with a fine bristle brush until it is evenly distributed and has neither gaps nor mountains. It is then wet with a spray bottle.

The water-wood glue mixture with a drop of dishwashing liquid dripped on with a small bottle afterwards can spread better and flow between the fine stones to fix them all equally. This process can be repeated several times if necessary. The only important thing is that the diorama is given sufficient time to dry afterwards.

Next, we work on all surfaces on which the spruces are to stand later, as well as a smaller surface in the foreground, which should look particularly puffed by the heat. Here we apply the coniferous forest soil of Busch for the track H0 (7529).

So that it doesn't look too coarse for Z gauge, we sift it out using Minitec screens, which were available at least before the brand was sold to Unique, in the largest mesh size. We sprinkle the forest soil into the grass glue bed (Still 61130) applied with a paintbrush and throw in any short cut sticks.

They come from a willow in our own garden, just like a tree trunk glued to the road. This material was sterilized in the oven at 180° C for 30 minutes, as was the East Frisian arable land (collected and sifted out by the farmers themselves). Only when the high temperature penetrates the entire material for several minutes does it destroy the proteins of living organisms in all conceivable stages.

Meanwhile our diorama with forest soil and soil application looks like fallow land in all other places. We are now beginning to change that. Even during extreme drought, there are always plants that look well soaked. They are often flat groundcovers that live in the shade of larger plants or thrive on a vein of water. We show them selectively with the medium green flockage of Noch (07242), which is scattered on the grass glue from the same firm.

Included in the wild growth, we also have individual parts of the field railway track bed, including a somewhat larger, continuous area near the level crossing. A few stalks should also sprout here. In order to keep the overgrown track gaps credible in relation to the low rail height, we use only 0.6 mm short grass fibres "Green 7" from Linea Secondaria (2A-7) from Italy.



The places where spruces are to be planted later are sprinkled with coniferous forest soil from Busch. Since it is intended for nominal size H0, we sift it with stackable inserts from Minitec. We randomly throw a few withered and dried pieces of willow branches onto the glue cover of the forest floor.

The subsequent landscaping of all other surfaces is carried out according to the "Kuhlmann method": the grass adhesive is not applied to the surface, but only dabbing, which in places always leaves a bare surface behind and is the only way to ensure that the meadow surfaces do not appear monotonous in terms of density and height.

The further procedure corresponds to the double grassing already described several times in this magazine, i.e., fibres of different lengths and colours are applied in separate passages and with dry pauses.

The first pass is made with 5 mm long, very dry field grass fibres in ochre brown from Noch (07086). The intermediate result, which you can see in our pictures, should not frighten you, because we are far from the end of the greening phase. The steppe landscape will only be of short duration.

Once it has dried, it continues with a summery green mixture. We quickly said goodbye to the much fresher and more realistic spring meadow grass (Noch 08300), because it did not harmonize with the field grass and the desired dry impression.



The grass fibres are shot in two steps with the Noch-Grasmaster 2.0. We start with the dry field grass (photo above), after drying the summer grass mixture follows (photo below). The grass glue is applied in accordance with the "Kuhlmann method", not over a large area, but dabbing.

Noch's master grass mixture Sommerwiese (summer meadow) (07072) with a fibre length of 2.5 to 6 mm, which was shot into the areas already partially covered with grass, but not limited to small areas, fit much better here.

Otherwise, we tend to reject such colour mixtures because they contain too much yellow for our taste, which we prefer to apply separately and self-dosed, and above all red stalks which have nothing in common with reality.

Strangely enough, it is precisely the grass mixtures that are most frequently bought by model railroaders. Perhaps we can help to change this behaviour for more realism?

Final fine sanding

Landscape structure and low vegetation are in place, with which we can now go to the finishing touches. We start by fitting the guardrail, which has to be bent towards the ground at one end before we can drill the holes for the brackets.

Also the traffic signs with St. Andrew's crosses, speed limits and lifting of road bans are now in the line. Heinz O. Schramm really did a great job here and created clearly legible traffic signs that give our diorama the final touch.

While the crash barrier has already been installed and protects cars from slipping onto the track bed, we are still waiting a bit with the signs glued into their holes. Before that we want to make the road look a bit used. Here soot-black powder paint of Kremer pigments helps, which we brush in cracks and small gaps of the tar layer and bind with the adhesion and fixing spray of Noch (61152).



Now crash barriers and traffic signs from HOS-Modellbahntechnik are used. The crash barrier to secure the difference in height between the road and the field railway line must be shortened and angled at the right end.

For the spruce trees we have decided for different models of Microrama, whose needling we have already presented in detail last year. So only the fly mushrooms are missing, which led to the choice of the conifers. They should be shown as true to scale, as possible.

This is only possible with track nails of gauge Z. These are hammered into a wooden batten, which can sometimes be crooked or crooked. Their heads are dabbed with a swab of epoxy resin glue (Uhu), which slightly rounds off this surface. After drying, a white spray primer to simulate the white stems is applied.



A matt and not too uniform appearance of the road can be achieved by finishing with powder paint. Using fine bristle brushes, it is worked into the cracks and joints that the coating shows. The pigments are permanently fixed with Noch's adhesive and fixing spray.

The caps can easily be set off with red paint and a small hairbrush, but it gets tricky with the typical dots. We gratefully reach for the pointed gluing tools that we once found in the model compass (Heinz Wagner), perfect, but too tiny!

A Zettie will certainly notice these almost scale jewels, but a trade fair visitor with a supposedly rather gross motor disposition and a limited focus on H0 gauge will find them very interesting. Without further ado, we decided to also produce some oversized specimens from N-gauge track nails according to the same scheme and added them.

Let them laugh, because maybe they will recognize the tiny ones right next to them. And then it's safe to swallow or even snap! After the fair, we decide, the big mushrooms should experience again soft, smaller replacements and perhaps times on a diorama of the children to move in, which are still on the way in larger scales.

Our crowning finale is to bring some life to the showpiece. As a commissioned work Trafofuchs produced a pair of seated hikers, which we place permanently on the lying tree trunk next to the road with instant binding glue in gel form.



So our diorama finally took part in the EM competition. But the question arises: Where is the fox? A little tip: Look for it in front of the conifer tree on the far left. Another highlight are the almost scale fly mushrooms (photo bottom left).

But the true hammer for us is a small forest animal, which despite its reddish coat colour is hardly recognizable in the undergrowth and proves how well it is actually camouflaged in the surroundings of a forest. The inventors of Creativ-Modellbau Klingenhöfer have perfectly implemented the dog-like predator (TF01) and provided us with an eye-catcher that also finds a permanent place here.

And so our diorama now also has a second name in the form of a challenging slogan: "Be a fox and find him". Well then, have fun searching! Fortunately, we are fine-motorically blessed mini model railroaders also equipped with a good sense of intuition...



Manufacturers of Materials used:

- <https://asoa.de>
- <https://www.bindulin.de>
- <https://www.busch-model.info>
- <https://www.conrad.de>
- <http://www.hos-modellbahntechnik.de>
- <https://www.klinghoefer.com>
- <http://www.lineasecondaria.it>
- <https://eshop.microrama.eu/de>
- <https://www.molto.de>
- <https://www.noch.de>
- <https://www.puren.com>
- <https://www.saller-modelle.de>
- <http://www.sceneryproducts.de>
- <http://www.trafofuchs.de>
- <http://www.molto.de>
- <https://www.uhu.de/de>

Vallejo-Farben with Trainini®-reader rebate:

- <https://www.modellbaukompass.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Schmalspur im Diemeltal **Vergessene Grubenbahn**

Heute ist es kaum noch vorstellbar, dass in Deutschland einst Eisenerz gefördert und nicht nur verhüttet wurde. Neben der Region um das westfälische Siegen war auch der Martenberg in Nordhessen, nicht weit von Brilon Wald (Nordrhein-Westfalen) und Waldeck (Kurhessen) entfernt, eine Quelle für den begehrten Eisenstein. Mit ihr verbunden war eine von Deutschlands unbekanntesten Eisenbahnen, die eine durchaus wechselvolle Geschichte erlebte.

Rolf Löttgers
Die Rhene-Diemeltalbahn Bredelar - Martenberg

Verlag im Biebortal, Rainer Haus
Biebortal 1990

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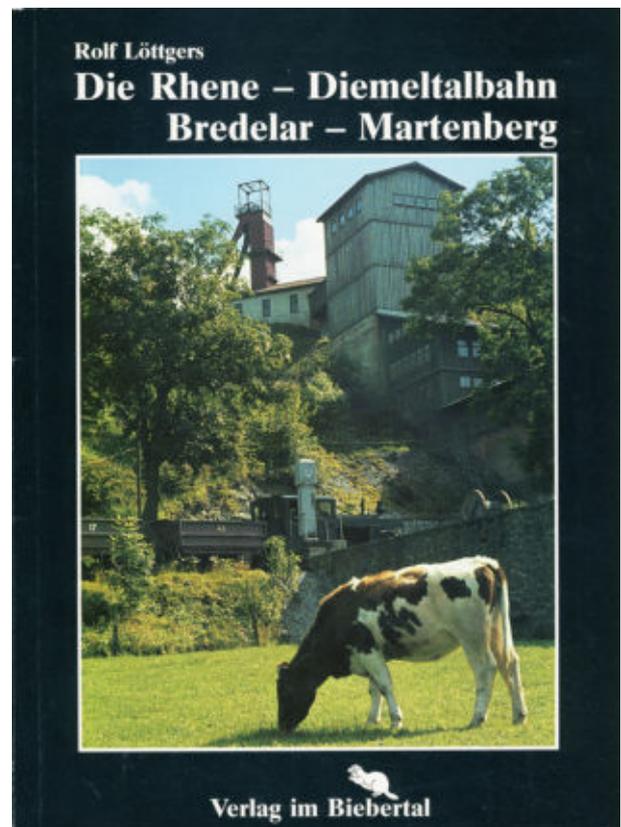
Erhältlich nur noch antiquarisch

„Die Rhene-Diemeltal-Eisenbahn gehört zu den unbekanntesten Privatbahnen, die in Deutschland überhaupt existiert haben. (...) Das hat weiter seinen Grund darin, dass die Bahn nur Güter-, aber niemals auch Personenverkehr gekannt hat, in den Kursbüchern der Staatsbahn also nie verzeichnet war.“

So beginnt der Siegener Autor seine Beschreibung der meterspurigen Bahn, die als Grubenbahn für den Erzverkehr in der Region nahe Waldeck (Kurhessen) geplant wurde und in einem strukturell schwach entwickelten Gebiet einst für Arbeit sorgte. Trotz der geringen Bedeutung der Erzvorkommen, deren Ausbeutung nur für sehr kurze Zeit wirtschaftlich interessant war, hat die kleine Bahn eine interessante Geschichte.

Sie hatte ihren Ursprung im Jahre 1872, konnte aber erst nach einigen Wechseln und Bereinigungen unter den Eigentümern der beteiligten Gruben gebaut werden. Eine bedeutende Rolle spielten dabei vor allem die Aktiengesellschaft Union (Dortmund) und die Aplerbecker Hütte (heute Stadtteil von Dortmund), die Abnehmer des Eisenerzes waren.

Die Betriebsführung war aufwändig, weil es damals noch kein preußisches Kleinbahngesetz gab und so die regulären Eisenbahnvorschriften galten. Außergewöhnlich für eine Schmalspurbahn mit 1.000 mm Spurweite war, dass die Fahrzeuge mit Regel- statt Mittelpuffern ausgestattet waren.



Bereits 1917 schien der kleinen Bahn mit dem Schließen der letzten Grube die Existenzgrundlage endgültig entzogen. Das geförderte Gestein wies zuletzt einen Eisengehalt von nur noch 20 % auf, was mit Blick auf die Kosten importierten Materials nicht mehr abbauwürdig erschien.

Mit neuer Spur von 900 mm auf politischen Druck durch Mannesmann wiederaufgebaut, erlebte sie ab 1937 eine Renaissance, die sich auch nach dem Krieg bis ins Frühjahr 1963 fortsetzte. Genutzt wurde die alte Trasse, die Anlagen wurden aber so einfach wie möglich gehalten. Grund war die gewünschte Selbstversorgung Deutschlands mit dem geförderten Rohstoff.

Rolf Löttgers weiß die Geschichte von Bahn und Eisenerzabbau trotz bescheidener Archivunterlagen gut zu erzählen. Untermauert hat er dies mit aufschlussreichen, historischen Aufnahmen. So wird aus dem dünnen Taschenbuch ein Werk, das nicht nur lokal Geschichtsinteressierte ansprechen kann.

Sein Werk kann auch beim Gestalten der Modellbahnanlage helfen: Wenn eine Schmalspur- oder Feldbahnstrecke integriert werden soll, ist guter Rat teuer. Da es solche Bahnen heute kaum noch gibt, ist es schwierig, glaubhaft wirkende Bahnanlagen nachzubilden. Besonders bei der Anlage minimaler, aber funktioneller Gleisbilder zeigen sich Probleme.

Aber auch die Streckenführung entlang von Straßen oder Flüssen, samt Überquerungen auf Brücken oder niveaugleicher Bahnübergänge, erfordert Fachwissen, weil hier durchaus wahrnehmbare Unterschiede zur Regelspur bestanden. Und gerade da liegt der Reiz, dergleichen in die Modellbahn zu integrieren.

Dabei kann dieses Buch eine wertvolle Hilfe bilden, besonders im äußerst interessanten Umfeld einer Grube – es muss ja nicht zwingend eine Großzeche aus dem Ruhrgebiet oder dem Saarland sein.

Zu einem Hingucker aufwerten lassen sich da Erbstollen oder ganz besonders Umladeeinrichtungen von der Schmalspur und Feldbahn auf LKW oder Regelspurgüterwagen. Passende Modellumsetzungen finden sich beispielsweise auf der Anlage „Sägethal“ von Götz Guddas oder an der „Schwerspatgrube Clarashall“ von Gerhard Maurer.

Viele Modelleisenbahner lieben doch das Besondere, denn gerne erleben wir Abwechslung beim Betrachten der Anlagen auf Ausstellungen. Schnöder Kreisverkehr um Vollmer-Häuschen lädt wohl niemanden ein, länger an einem Arrangement zu verweilen und Details zu studieren.

Dieser antiquarische Band gehört zu denjenigen, die erforderliche Anregungen liefern und Gespür für eine korrekte und vorbildnahe Umsetzung liefern können. Es lohnt sich, sich mit der Materie abseits der vollspurigen Eisenbahn auseinanderzusetzen.

Museumsbahnen Nordrhein-Westfalens Zur statt mit der Bahn in den Urlaub

Der Sommer lockt mit warmen Temperaturen und Sonnenschein zu Ausflügen. Eisenbahnfreunde möchte aber auch im Urlaub nicht auf ihr Steckenpferd verzichten. Bei der VG Bahn ist kürzlich ein Buch erschienen, dass sich als perfekter Reiseführer für Ausflüge zu nostalgischen Eisenbahnen im bevölkerungsreichsten Bundesland eignet. Warum also nicht mal woanders und auf andere Weise den Urlaub genießen?

Korbinian Fleischer
Reiseführer Nostalgiebahnen
Nordrhein-Westfalen

VGB Verlagsgruppe Bahn GmbH / Klartext Verlagsgesellschaft mbH
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Sechs Jahre nach Erscheinen eines ersten Titels dieser Reihe zu den bahntechnischen Sehenswürdigkeiten Baden-Württembergs folgt nun rechtzeitig zur Sommersaison 2019 ein Band für Reiseziele in Nordrhein-Westfalen.

Auch im bevölkerungsreichsten Bundesland lassen sich heute immer noch technische Zeitzeugen längst vergangener Zeiten auf den von Museumsbahnen befahrenen Strecken in Aktion erleben.

Als Besonderheit im Sauerland, im früheren Aachener Kohlrevier und ebenso im Ruhrgebiet vermitteln diese Bahnen ein ganz besonders Bewusstsein: Ohne die Eisenbahn wäre der industrielle Aufschwung vieler Regionen gar nicht möglich gewesen.

So sind sie Bestandteil einer ganz besonderen Industriekultur, die es verdient hat, in einem Buch zusammengefasst und katalogisiert zu werden. Die Idee von Autor und Verlag passt umso besser, als das sich Nordrhein-Westfalen und besonders das Ruhrgebiet seit Jahren an steigendem Tourismus, ablesbar an den Übernachtungszahlen, erfreuen.

Das ganz besondere Flair, wie technische Denkmäler und Industriekultur hier Eingang ins tägliche Leben und die Stadt- wie auch Landschaftsbilder gefunden haben, ist wohl einmalig. Da ist es sicher kein Zufall, dass der zweite Band einer sicher noch weiter fortgesetzten Reihe diesem Bundesland gewidmet worden ist.

Eingerahmt von einem Grußwort des VDMT-Präsidenten zu Beginn und abschließenden Erläuterungen zur „standesgemäßen“ Anreise mit der Bahn, Blicken hinter die Kulissen der Museumsbahnen



(Aufarbeitung und Instandhaltung von Strecken und Fahrzeugen) und einer Übersichtskarte der Bahnstrecken finden sich Portraits und Vorstellungen folgender Museumsbetriebe:

- Museumsbahn Rahden – Uchte
- Museumsbahn Minden – Hille und Bohmte – Preußisch Oldendorf
- Landeseisenbahn Lippe (Extertal-Bösingfeld – Barntrup – Dörentrup)
- Eisenbahn-Tradition (Lengerich und Osnabrück – Mettingen)
- Dampf-Kleinbahn Mühlenstroth
- Museumseisenbahn Hamm-RLG – Lippborg-Heintrop
- Sauerländer Kleinbahn (Hüinghausen – Köbbinghauser Hammer)
- Bergische Museumsbahn (Kohlfurth Brücke – Möschenborn)
- Hespertalbahn (Essen-Kupferdreh – Haus Scheppen)
- Ruhrtalbahn (Hagen – Hattingen und Hagen – Ennepetal-Altenvoerde)
- Hist. Schienenverkehr Wesel (Rheinpromenade Wesel – Hohe Mark)
- Krefelder Schluff (St. Tönis – Hülser Berg)
- Selfkantbahn (Gillrath – Schierwaldenrath)
- Millionenlinie (Kerkrade-Centrum – Simpelveld – Valkenburg – Vetschau)
- Eifel-Bördebahn (Düren – Zülpich – Euskirchen)
- Oleftalbahn (Kall – Hellenthal)
- Pützchensmarkt-Express (Bonn-Beuel – Pützchensmarkt – Hangelar)
- Wiehltalbahn „Bergischer Löwe“ (Dieringhausen – Wiehl – Waldbröl)
- Rheinisches Industriebahnmuseum Köln

Jeder der bundesweit bekannteren oder auch nur regional verankerten Bahnen sind mehrere Seiten im Buch gewidmet, die über Anfahrt, Programm, Geschichte von Bahn und Museumsbetrieb sowie die eingesetzten Fahrzeuge informieren.

Als kleine Besonderheit haben wir die Sauerländer Kleinbahn herausgepickt, die mit dem Titelfoto bedacht wurde. Sie ging nicht direkt aus einem Regelbetrieb hervor, sondern wurde nach Stilllegen einer Bundesbahnstrecke als Museumsbetrieb gegründet und baute auf den früheren Trassen einer Meterspurbahn auf. Ihr Ziel ist es, das Erbe der vielen Kleinbahnen im Sauerland zu bewahren.

Der vorliegende Reiseführer zu den Nostalgie- und Museumsbahnen Nordrhein-Westfalens bietet in Summe eine wohl vollständige Übersicht für alle Bahnfreunde, die sich auf die Spuren früherer Dampfeisenbahnen begeben wollen.

Wer den Sommer oder auch die kalten Tage mit prächtiger Dampfentwicklung für Eisenbahnurlaube oder Ausflüge nutzen möchte, findet hier den passenden Begleiter. Hinweise auf Fahrtage samt Streckenkarten/-übersichten sowie Empfehlungen für die ganze Familie runden dieses Buch ab.

Publishing pages with reference possibility:
<https://shop.vgbahn.info>

Eisenbahnmuseum Dieringhausen



Einzigartig in Europa

*Spur Z Ausstellung
im historischen Güterwagen
im Eisenbahnmuseum Dieringhausen*

Die Sonntags-Ausstellungstermine:

*08.06.2019, 09.06.2019, 16.06.2019, 07.07.2019,
28.07.2019, 11.08.2019, 22.09.2019, 13.10.2019,
27.10.2019*

jeweils von ca. 10:00 Uhr – ca. 17:00 Uhr

Infos unter: www.stammtisch-untereschbach.de

Excursion to the DB Museum Nuremberg

Beer, bananas and more

The world's oldest railway museum is located in Nuremberg and has been welcoming visitors almost every day during the past 137 years. Its collection is diverse and its conceptual approach to exhibitions is changing. If you like railways, this is the place to visit. Until 31 October 2019, there will also be a special exhibition. Let us take you on a tour of the DB Museum Nuremberg.

Founded in 1882, the Nuremberg Transport Museum, renamed DB Museum after the privatisation of the German Federal Railway, is the oldest railway museum in the world.

The museum welcomes more than 200,000 visitors every year, and offers a first hand experience from various angles of the history of the German railway. In addition to its main facility in Nuremberg, the museum has two additional locations in Koblenz and Halle (Saale), which focus on special topics and regularly organise their own events.



The DB Museum, which emerged from the Transport Museum for Railway and Post, is the oldest railway museum in the world. Photo: DB Museum / Mike Beims

For some years now, the museum has been operating under new management and under the umbrella of the Deutsche Bahn Foundation, which has obviously helped to increase its attraction. In comparison to previous visits, some sections appeared visibly fresher and more modern, and thus more appealing, especially to younger visitors.

The core collection of the Nuremberg Museum includes epoch-making items such as the saloon cars of King Ludwig II and Chancellor of the Empire Bismarck, a replica of the "Adler" (Eagle) and the original of the class 05 001, as well as models of the Royal Bavarian State Railway in 1:10 scale.



A classic among the Nuremberg exhibits is the famous saloon car of Ludwig II, King of Bavaria.

We owe it in particular to this large collection of scale models that we preserve the knowledge of the livery and colours used in this specific 19th century railway administration.

All these scale models are originals from that period and their age alone already deserves them a place in the exhibition.

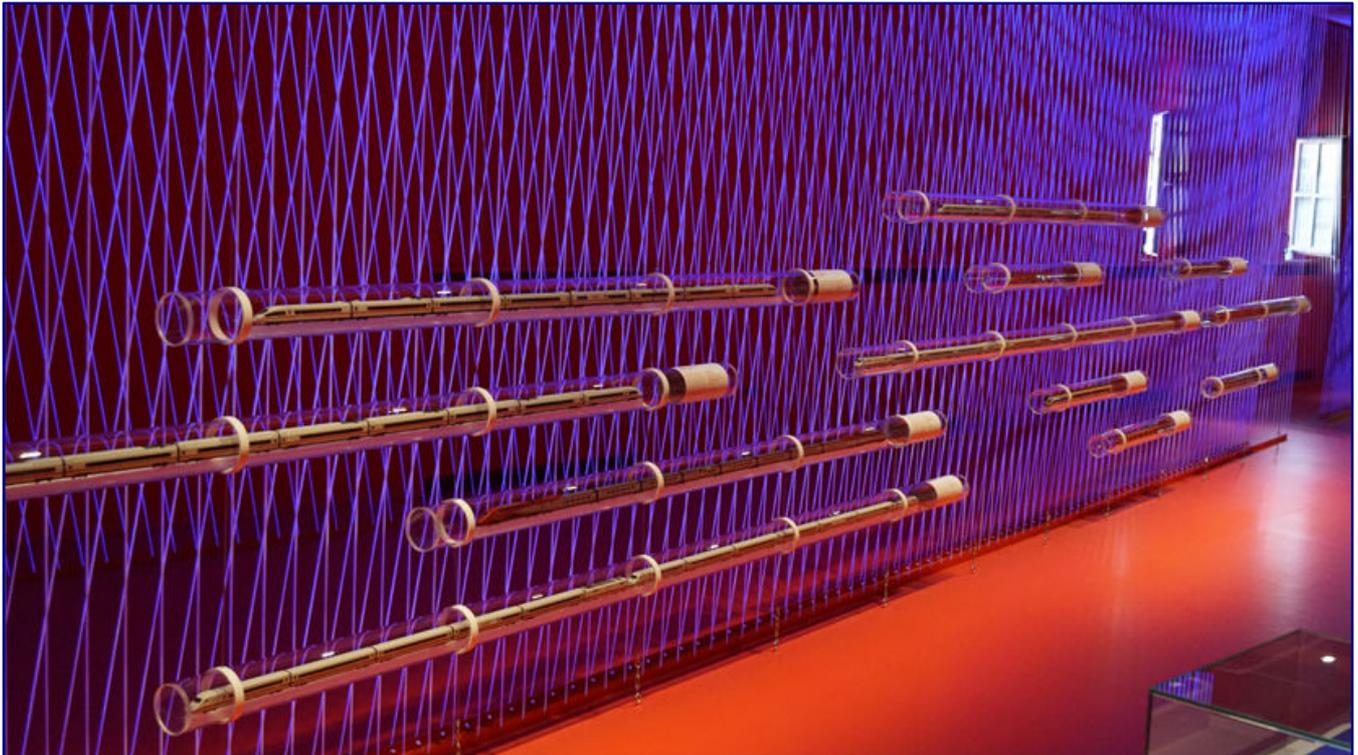
All in all, the exhibits keep alive German railway history throughout all of its periods. This also includes dark times of history such as the participation of the Reichsbahn in deportations and war.



The "Flying Hamburger" in the shape of a SVT 877. Unfortunately, only one section of this diesel rail car remains, albeit in excellent condition.

The museum's honourable halls present visitors with an accessible account of railway history from its beginnings around 1800, including two original cars from the 19th century, to the present day.

A particularly exciting part of the museum is its new permanent exhibition "On separate tracks: the railway in divided Germany from 1945 to 1989", which opened on 8 June 2019 and shows the differences, but also parallels between the two German railway systems during the four decades after the Second World War. The TEE routes in Western Europe served by the West German class VT 11⁵ and the East German DR Class VT 18.16 railcar make for a quite an illustrious and interesting example in this respect.



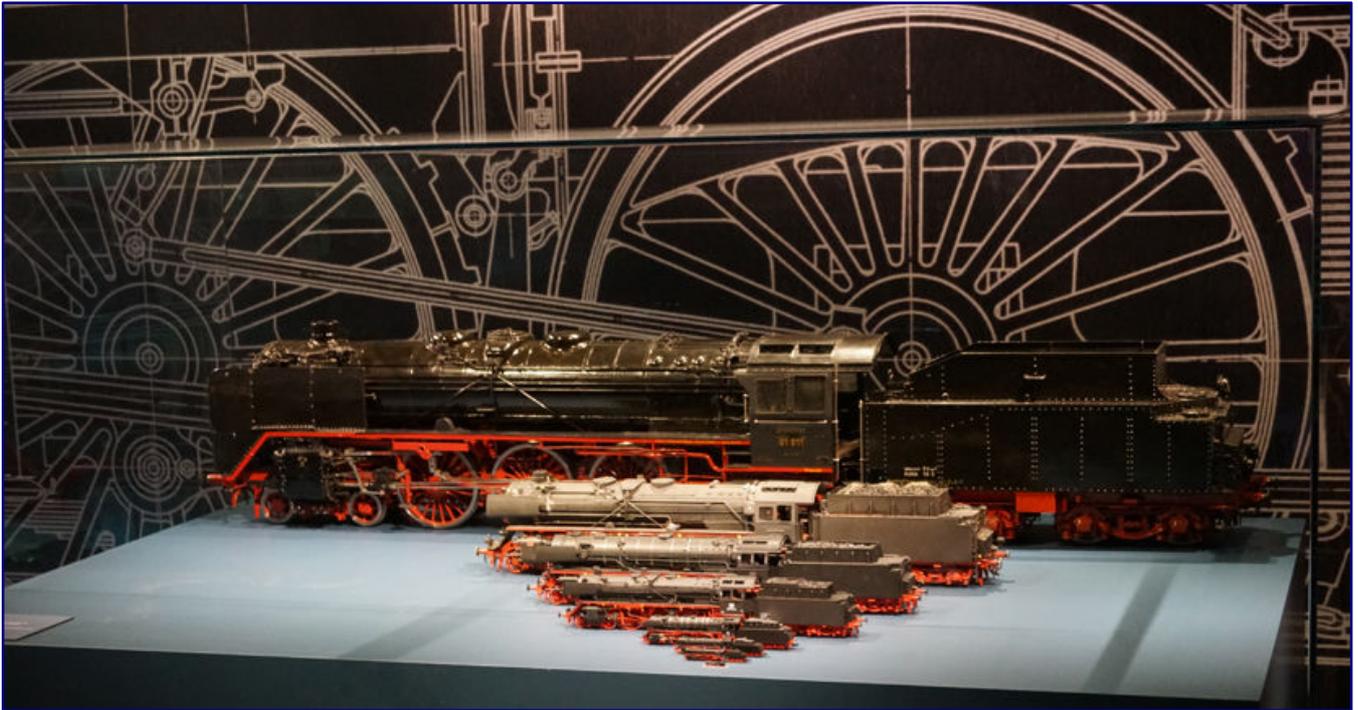
The exhibition on the future of railways impresses with clever lighting and visual effects. Part of this presentation are also models of the high-speed trains that are in service on the German railway network.

Younger children also get their money's worth in Nuremberg at the "Kibala" (Children's Railway Land) devoted entirely to them on the 2nd floor of the museum. Here they can experience railway history in a playful way in the truest sense of the word and even dress up as typical railway employees or take a trip across the entire floor on a large 5-inch gauge train.

Kids will receive a free ticket at the box office, and parents will find a place for taking a break and recharging their batteries.

The people in charge of the museum obviously also wanted to set an example with covering the future of train travel. Focusing on digitisation, networks and high-speed train technology, the "Future" section of the museum makes use of illuminated displays in otherwise darkened rooms, clever visual presentations and advanced lighting techniques.

Similar exhibition techniques add to the attraction of the model railway displays in the "Modellarium" section on the 1st floor of the museum. Cool but bright lights highlight the display cases and their contents of various modelling scales against the surrounding relative darkness.



Right in the entrance area of the railway models exhibition section we find the class 01 high-speed steam locomotive in all conceivable model railway scales (top). A scale 1:10 replica of the V 80 001 is also on display in this part of the museum (bottom).

Amongst the models on display are more recent scale 1:10 ones from DB times, a large steam locomotive 23 001 as well as many other exhibits from various model railway scales down to a T gauge ICE model.

One large display case is exclusively dedicated to N gauge models. As for Z scale, we were particularly struck by the direct comparison of scales and track gauges on the basis of a class 01 steam locomotive. This seems to be the only locomotive that has been produced across all modelling scales.



A modelling classic is the museum's large Fleischmann H0 layout which is operated several times a day. Whilst the layout looks somewhat dated, compared to today's expectations and possibilities, it is nevertheless an item not to be missed. It impresses with its sheer size, realistic track layouts and a prototypical operating system. Once cutting edge technology, it does represent a milestone in model railway engineering.

The main reason for our most recent visit, however, was the special exhibition "Beer, Railway and Bananas" on the outdoor grounds, which has been running since 12 April. Amongst the two permanent signal boxes (allowing kids to operate different types of semaphore signals), the DB Museum has brought together nine freight cars and one passenger car as well as two freight locomotives (E 91 99 and 45 010).



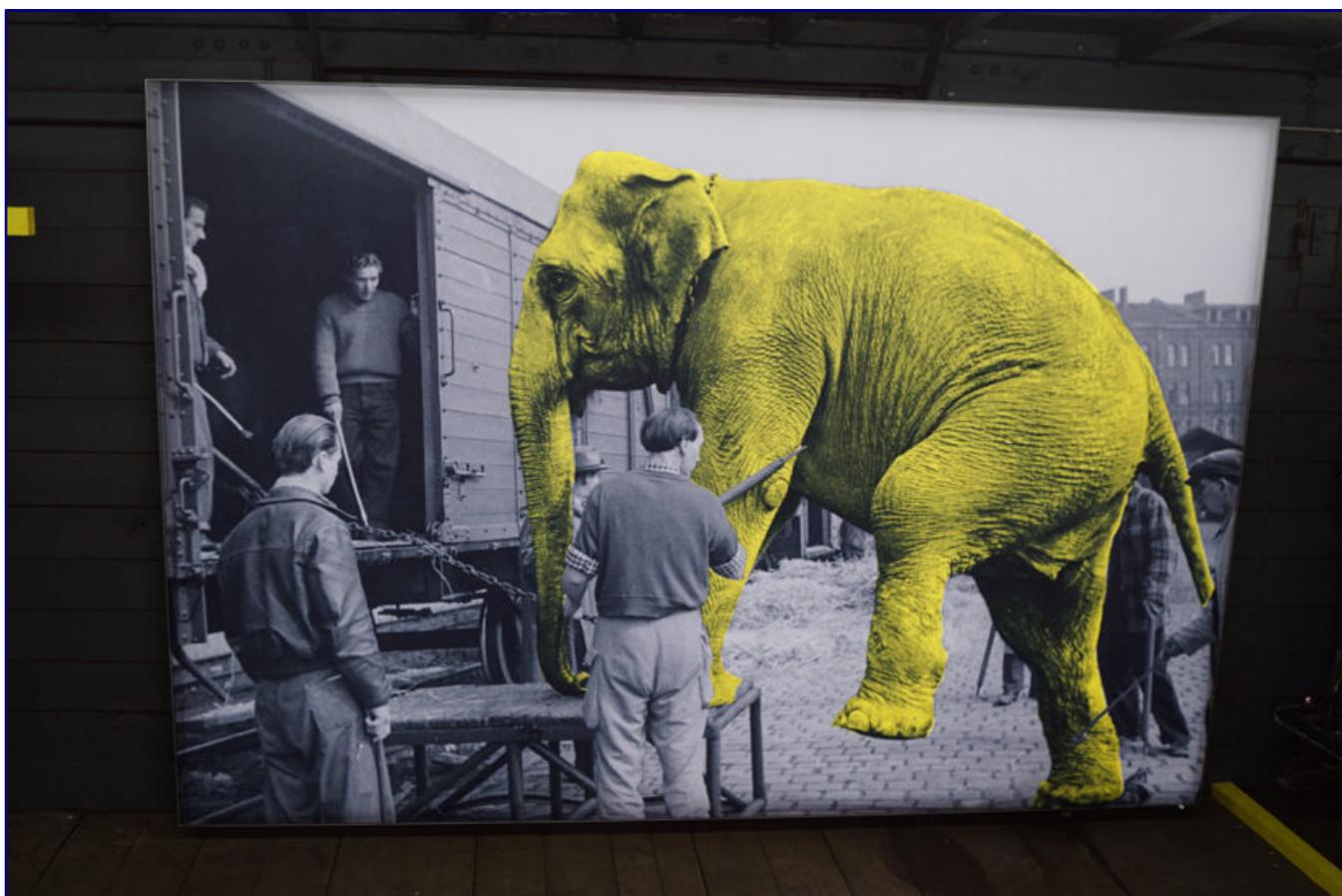
Since 12 April and until 31 October 2019, the DB Museum has been running a special outdoor exhibition entitled "Beer, Rail and Bananas". This event deals with the colourful facets of freight transport - starting with the transport of beer kegs on the Adler train in 1836.

They give visitors the opportunity to see and understand the many facets of freight transport by rail and to understand its development, starting with what began on 11 June 1836 - and not, as often assumed on 7 December 1835 - by transporting two barrels of beer brewed by Georg Lederer from Nuremberg's Plärrer railway station to the innkeeper at Fürth's station.

This marked the beginning of rail freight transport in Germany. In the following decades railways became the most important means of transport and a driver of Germany's industrialisation already at the end of the 19th century.

Some of the cars on display have been opened and equipped with partitions. They show the development from bulk cargo to container transport, the transport of foodstuffs such as bananas or beer in covered tank wagons or even curiosities such as the transport of animals, illustrated with a photo of a circus elephant. By selection of cars presents an arc from 1835 to the more recent past.

Continued on page 53



The refrigerated and covered freight cars at the loading ramp are accessible (top) and display images of some curious rail transports, such as that of circus elephants (bottom).



An exhibit in the goods shed, showing historical tools which were used to handle bulk materials at railway stations throughout the long history of railways.

Further exhibits are on display in the adjacent goods shed. They mainly deal with the problem of how to handle and load bulk materials into covered freight cars.

From the historical wheelbarrow, the "Ant" for moving pallets to other types of tools for moving and lifting cargo, the main items and their development over time of this particular freight handling challenge are shown here.

If this report has whet your appetite for visiting the Transport Museum, please know that it is suitable not only for rainy days, as most of the special exhibition is outdoors.

A thorough visit of all the sections of the museum will take at least four hours, so it is best to come with some peace and quiet. But time is pressing, at least if you want to see the special exhibition "Beer, Railway and Bananas" which runs only until 31 October 2019.



Part of the special exhibition on the history of bulk cargo transport is also 45 010, the last remaining specimen of Germany's heaviest freight steam locomotive.

Opening hours and prices:

https://www.dbmuseum.de/museum_en

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

This reader is pleased about the June issue:

This issue was a very special highlight for me. The report "Landscapes and Tracks" was for me, as a searcher for a possible plant form, stimulating, as well as unique.

So I'm very excited about the sequel! Also, tool tests are helpful again and again, so the presentation of the locomotive service tray from Noch.

It's great what this little editorial office conjures up month after month! I've wanted to tell you that for some time now. I wish you much success and enthusiasm in continuing this little gem Trainini!

Dietrich Frank, Düsseldorf

Märklineum stays open later:

The Südwest-Presse (SWP) announced on 3 July 2019 that the new Märklineum will not be fully opened until later than planned. The new factory outlet has been open since September 2018, but the showroom and company history are still a long way off. Most recently, the Märklin pages spoke roughly of the first half of 2020.

Now it was confirmed what had been suspected for some time, because the date is slipping far behind. The newspaper referred to press spokesman Eric-Michael Peschel and named May 2020 as the new date. The museum will not be ready for the Märklin Days in September 2019, but visitors should, at least, be able to get a glimpse of how the exhibition will be set up.

The reason given for the delays was that an intermediate ceiling had to be removed for the exhibition. Märklin doesn't want to break a knee there.

Retrofitting kits from Z-Railways:

With the V 36 Märklin has created a beautifully constructed locomotive, which is characterized by excellent running characteristics and attractive appearance. But in all versions it has not remained without compromises, as our test had already shown.

This also means that it offers potential for upgrading and fine detailing. After the green DR version (Item No. 4017-19093071) had already been considered, Z-Railways (<https://www.shop.z-railways.de>) now also offers an upgrade kit for the Bundesbahn version with raised cockpit (4017-19103111).



It was developed and produced by eXact-Modellbau in Bad Doberan, but is exclusively available from the small series distributor. It consists of two cast brass typhoons, with which the missing part on the front wall of the cab can be retrofitted, a foldable ladder with climbing protection and the missing shunter handrails on the front ladders of all Märklin models.

Also the coupling rods can be exchanged against finer and one-piece etched parts, which belong likewise to the scope of the delivery. We would like to supplement at this point also that also further kits of eXact-Modellbau are in the delivery program: finely etched stage railings for Märklins Donnerbüchsen, conversion kits for thunder rifle command wagons and the hay wagons developed already some years ago (two copies per kit).

Exclusive innovations at Archistories:

Archistories was busy again and announced several innovations. The first two are produced exclusively for Ztrack-Center. It is an expandable warehouse complex in the typical North American style.



Already the basic construction kit of the warehouse "Railway Express Agency" (Art.-No. 103191), produced exclusively for **Ztrack**, results in an impressive building. Photo: Archistories

The REA-Transfer-Warehouse (Art. 103191) is a warehouse of the Railway Express Agency and, together with the extension kit (104191), makes it possible to display elongated freight and loading areas along the tracks. The possible dimensions of such a building are impressive, as the corresponding photo shows.

The kit is at least theoretically infinite, in practice in any case arbitrarily expandable. Its strengths lie in its mature and warp-free construction as well as the many small details.



With the extension sets (104191) the complex can be extended at will, theoretically infinitely. Photo: Archistories

These include a filigree external staircase, the optional building lettering, elaborately constructed windows and even movable gates. The dimensions of the basic kit are 80 x 155 x 80 mm (L x W x H), those of the extension 80 x 91 x 72 mm.

These two kits can only be purchased via <https://ztrackcenter.com>

The 1zu220 shop also exclusively produces two other new products which are expected to be available from the end of August 2019. They continue the popular series of buildings in local architectural styles, most recently supplemented with a village church.



Here you can see the backside of the fire station Dörpede (402191) with the attached crate. Photo: Archistories

The fire station "Kallental" (401191) shows a massively bricked sandstone base zone with a plastered framework in the upper floors.

This combination gives the functional building an almost romantic touch. The inserted hose tower underlines the use of the building with village character.

On the back side, a flat storage crate with a fine wood engraving enlivens the appearance. The two vehicle garages with large windows can be effectively illuminated and offer plenty of space for the fire engines to be stored here.

With the fire station "Dörpede" (402191) a counterpart is offered again, with which the framework is bricked up. The warm shade of the brick ensures a harmonious overall effect. In the other features, it follows the kit described above.



The dimensions of both village fire stations are 83 x 56 x 92 mm (L x W X H). Like all Archistories kits, they are made of high-quality, solid-coloured hard cardboard and do not require any further colour treatment.

In addition to the fire stations, the 1zu220-Shop (<https://www.1zu220-shop.de>) also offers a sound module "Siren" (NO-SOUND-SIRENE) for operation on a commercially available model railway transformer, with which the volunteers can be called in if necessary.

So that this can also be perfectly represented in the model, only the buyers of one of the two fire brigade kits can also purchase a Magirus-Deutz LF (WM-FEUERWEHR) fire fighting vehicle, also exclusively manufactured by Wespe Models. This model can also be seen on the title page and in our contribution to the summer diorama.

News about Märklin deliveries:

According to Märklin's schedule, some new products were about to be delivered at the editorial deadline. The following models were already available in time to discuss them here.

The series starts with the passenger steam locomotive class 24 with cross-country tender (art. no. 88031). Its model 24 061 was equipped with the tender 2'2' T 26 of the 50 909 and the Krauss-Helmholtz steering frame of the 64 512 in 1948 for the test office Göttingen (later Minden). On the one hand, this should increase the range and improve the running characteristics, especially when reversing.

Its miniature is equipped with a stationary bell-shaped armature motor with flywheel mass and shows a direction-dependent LED tip lighting in warm white at the front. The locomotive has no steam bell and benefits from a modified tender coupling that brings it much closer to the driver's cab. This is very good for the model's appearance.

It is astounding that after a slight reworking of the shape after a minor accident, the missing step under the smoke chamber has now been repaired, as has the standing boiler bulge removed during the same process (mould was a H0 derivative). This suggests the assumption that there must be at least two die casting moulds for the 24 series produced in large numbers.

Coal traffic on the Z-gauge plant can now also begin. The four-part freight wagon pack (86307) with three OOtz 50 wagons and one OOtz 43 wagon including loads was also delivered.

In addition, the self-unloading wagon OOtz 50 (86308) also rolls to the dealers. It also bears the side address "Erz IIIId" and also has a coal load insert. A second extension is the OOtz 43 (82803) self-discharging wagon in a comparable design and construction.

The car transport wagon DDm (87093) is in far blue paint and with modified Ege cookie (old DB logo) to the transition from epoch IV to V assign, suitable passenger coaches are not offered at the same time however. The double-decker wagon is also delivered unloaded.

Many Zetties have been waiting eagerly for the tender steam locomotive of class 86 (88962). After it had already undergone a model upgrade and since then has appeared with fully functional rods, replicas of the brakes and lamp brackets, only the engine technology had to be revised.

The newly delivered version as 86 234 of the Deutsche Bundesbahn (German Federal Railroad) with a written out owner's name also features the bell-shaped armature motor. Apparently, also the current decreasing wheel grinders were improved, which was a known weak point of the models.



Märklin's Bundesbahnlok 86 234 (Art.-No. 88962) with detail control and now also bell-shaped armature motor can be seen here together with the beer advertising wagon "Westheimer Weizen" (98165) from the 1zu220-Shop, also produced by Märklin.

Window inserts would have been desirable, but were not obstructed. Top lighting is provided by warm white LEDs. The colour scheme and printing have been successful: the cables below the driver's cab are individually set off in fire red, the lettering also includes lightning warning signs. However, the edges of the smoke chamber were not red.

Beer wagon series continued:

The 1zu220 shop (<https://www.1zu220-shop.de>) has continued its Westheimer beer car series with a sixth model. Available since 25 June 2019, the latest model of a thermally insulated freight car G 10 with the special print "Westheimer Weizen" (Art. No. 98165) is now available in a unique edition of 100 copies.

The basic colour of the model is RAL 9002 grey-white and thus follows the DB painting regulations for refrigerated wagons of model era IV. The roof colour of this extremely attractive design is RAL 5025 Perlenzian. The unprinted car bottom is unprinted and sprayed in RAL 9005 deep black (see photo on page 58).

The new products from AZL in July 2019:

Main topic of the beginning high summer is the passenger train "The Eagle" of the Missouri Pacific.

AZL serves it with appealing grey-blue painted light passenger coaches (Empire-Builder colour scheme) with yellow set-off stripes. A matching train locomotive in the form of the EMD E8A (Art. No. 62610-1 to -4) has been in the range for some time.

We search in vain for a suitably designed dining car in the delivery list, but at least the following car types have been considered: sleeping cars 6-6-4 (73114-1 / -2), viewing cars (73414-1 / -2), post-pack cars (73614-1), seat cars (73714-0), pulpit cars (73814-1) and rail mail cars (73914-1 / -2).

The 40-foot long, covered AAR freight wagons from 1937 now bear striking logo lettering "New York, New Haven & Hartford" on their brown lacquer and are available individually (904308-1) as well as in sets of two (904378-1) and four (914308-1).

You will find further manufacturer photos of the current deliveries under <http://www.americanzline.com>.

And here is what's new at Micro-Trains:

The second US large-scale series manufacturer is planning to deliver the EMD SD40-2 diesel-electric locomotives in the colours of the merged BNSF (Art. No. 970 01 291 / -292) this month. So far, however, only graphic representations of the model design have been shown.

Wagon number 6 (518 00 760) continues the series "Farm to table." The basic colour is brown, the side walls of the wooden 40-foot refrigerated wagon are set off in yellow and bear advertising logos for "Mrs. Tucker's Shortening."



Observation wagon (Art.-Nr. 73814-1) and the EMD diesel locomotive E8A of the Missouri Pacific (62610) which fits to the "The Eagle." Photos: AZL / Ztrack

40-foot standard cars with one-piece sliding door are the covered copies of Norfolk & Western (500 00 066 / -67) with black paintwork and white lettering. In addition there are the covered bulk wagons with two outlet openings of the Cotton Belt (531 00 291 / -292). (<http://www.case-hobbies.de>).

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