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Märklin's Pig Snout

Dortmund makes an Impression
Winter's sudden Return

Introduction

Dear Readers,

Finally, it's time to tinker again! Even if it was only enough for a short article this time, we are making progress with our beginners' winter layout at last. The goal is ambitious, as it should be on display at our anniversary exhibition at the beginning of October, if possible.

A few weeks later, it will be in demand as a store's window decoration, which I am particularly pleased about. With it, we will be able to bring the model railway back to Dortmund's pedestrian zone.

These are places where we used to press our noses against windowpanes, as children. Model railways still fascinate us today, but they have disappeared far too much from our everyday lives. We want to, must and can change that.

Our exhibition to mark the 20th anniversary of the magazine should be one step in this direction. To achieve this, we are planning a very colourful and varied programme that will appeal to all living generations, even if you are not already a model railway fan.

In the meantime, we have set up a separate page for you on our portal pages, which we will maintain and add to. Soon there will also be a list of exhibitors. But I promise you: We will not disclose everything! We are still good for surprises. And, if you don't come to Wasserkurl, you might regret it later.

But back to the present, the merry month of May. Last month, the Intermodellbau trade fair was held in Dortmund and Z gauge was conspicuously strong and well represented there. We haven't experienced this under old 'management' for a long time. Our report is correspondingly detailed.

And there were also plenty of new products from Märklin recently. We test the DB VT 88 902 Wismar railbus in detail and can also point out minor areas for improvement. But, among the other deliveries we also find some that are not just covered in one report.

We experience product maintenance and design variants, high-quality realisations, and, also, special features that no one seems to have noticed yet. That is why we have taken the time to cover all of this in detail and have come up with more material than we expected.

Our book tips do not (exceptionally) fit in with the topics you will find in the other reports. So, this time they expand the range of topics by themselves. What they have in common is that they are written in a very comprehensible, detailed and entertaining way and deal with highly interesting aspects.

If you were to ask me which of the two you should buy, I would have to answer: both! Let yourself be carried away and enjoy reading this edition, hopefully in early summer weather. We'll see you on 4 and 5 October, won't we?

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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Cover photo:

There were not many opportunities to take photos of VT 88 902 during its short period of service with DB. Here, it has come to a halt at the Wilhelmsthal stop in summer weather; passengers cannot be seen.

Wismar railbus from Märklin

Beauty with minor Flaws

To claim that the Wismar railbus was one of the Zetties' favourite models is perhaps a little exaggerated. After all, Z-Modellbau delivered a model years ago that set new standards. But since this manufacturer ceased trading, the calls became louder, and Märklin finally acknowledged them. Today, the Göppingen realisation has to prove itself in the test and also be compared a little with the model from the small series.

Their characteristic engine bonnets gave the vehicles their unmistakable appearance and have given them well-known nicknames to this day. The Wismar rail buses were affectionately referred to as Piggy's Snouts or Anteaters.

As they are not only popular with railway fans, but are also simple and robustly built, at least eleven examples from the Wismar railcar factory have survived to this day. In view of the short 'Hannover Type B' (for DRG, DR and DB) that appeared years ago at Z-Modellbau, and the new Märklin 'Hannover Type A' model, let us first take a look back in history.



The T 5 of the Wittlager Kreisbahn is the former DB VT 88 900, which was identical in construction to the Märklin model. In 1970, it can be seen in Wuppertal-Oberbarmen without the roof rack and with a different compressed air horn. It had previously been acquired by the Interessengemeinschaft Historischer Schienenverkehr. Photo: Bernd Backhaus, Eisenbahnstiftung

The type designations just mentioned (and another three) were only introduced later by the manufacturer, the Wismarer Waggonfabrik, in order to classify the vehicles and be able to designate them precisely. Nevertheless, hardly any of them were the same, as many changes were made to the interior design, track gauge, wheelbase, motorisation, external attachments, and also the car body.

They all represent railbuses of the Hannover type (named after the first customer), but the type designations A to E (A to C for standard gauge / D & E for narrow gauge) cannot even rudimentarily delineate the differences. They were developed as lightweight vehicles for branch line operation, which only offered 3rd class.

As early as the 1920s, Germany's light railroads came under enormous cost pressure on underutilized lines. Operation with steam locomotives proved to be too complex and expensive, while diesel traction was still in its infancy and was far from fully developed.

Waggonfabrik Wismar therefore borrowed from bus construction but had to consider the lack of reversing gears for equally high speeds in both directions. So, at the beginning of the 1930s, it developed this simple, but robust, railbus with two independently operating engines for each direction of travel.



OHE railcar DT 0508 originates from the 1937 delivery series and presents itself to the BDEF special trip participants in Winsen an der Luhe on May 22, 1966. One of the advantages of the railcars, affectionately known as "Anteaters" or "Piggy Snouts", was their ease of maintenance. This also meant that spare parts were available from any Ford dealer. Photo: Helmut Röth, Eisenbahnstiftung

They were housed in snouts that were attached at both ends, which provided easy access for maintenance and repairs. Initially, 40 hp petrol engines from Ford were used, later also diesel units from Deutz. The associated gearbox had four forward and one reverse gear.

The vehicles thus cost only about half the price of an ordinary railcar and became interesting for small and private railroads. According to contemporary advertising, they covered their costs with just six passengers.

The first prototype went to the Lüneburg-Soltau small railway in 1932 and proved its worth there. For this reason, the Hannover State Small Railway Office ordered a series of nine units for various small railways, which led to the aforementioned name Hannover.

By 1936, the range had grown to five basic types, but these were rarely supplied to standard. The individual wishes of the customers were always taken into consideration. Regardless of the track gauge, two different car bodies were used.

In the narrower version, the side walls were straight, while the wider version, to which the Märklin model belongs, has adapted, i.e., angled walls in the entrance area. The width in the seating area was always the same.



The upgraded driver's cab of a Wismar railbus built in 1933 with gearstick and foot pedals clearly shows that it is technically just a bus on rails. Photo: Jacek79 (CC-BY-3.0)

The 'Anteaters' were built until 1941, with a total of 57 units, plus two trailer cars. A further 25 railcars and three trailer cars went to Spain, and 14 units for the wide gauge were built under licence.

Märklin's original prototype

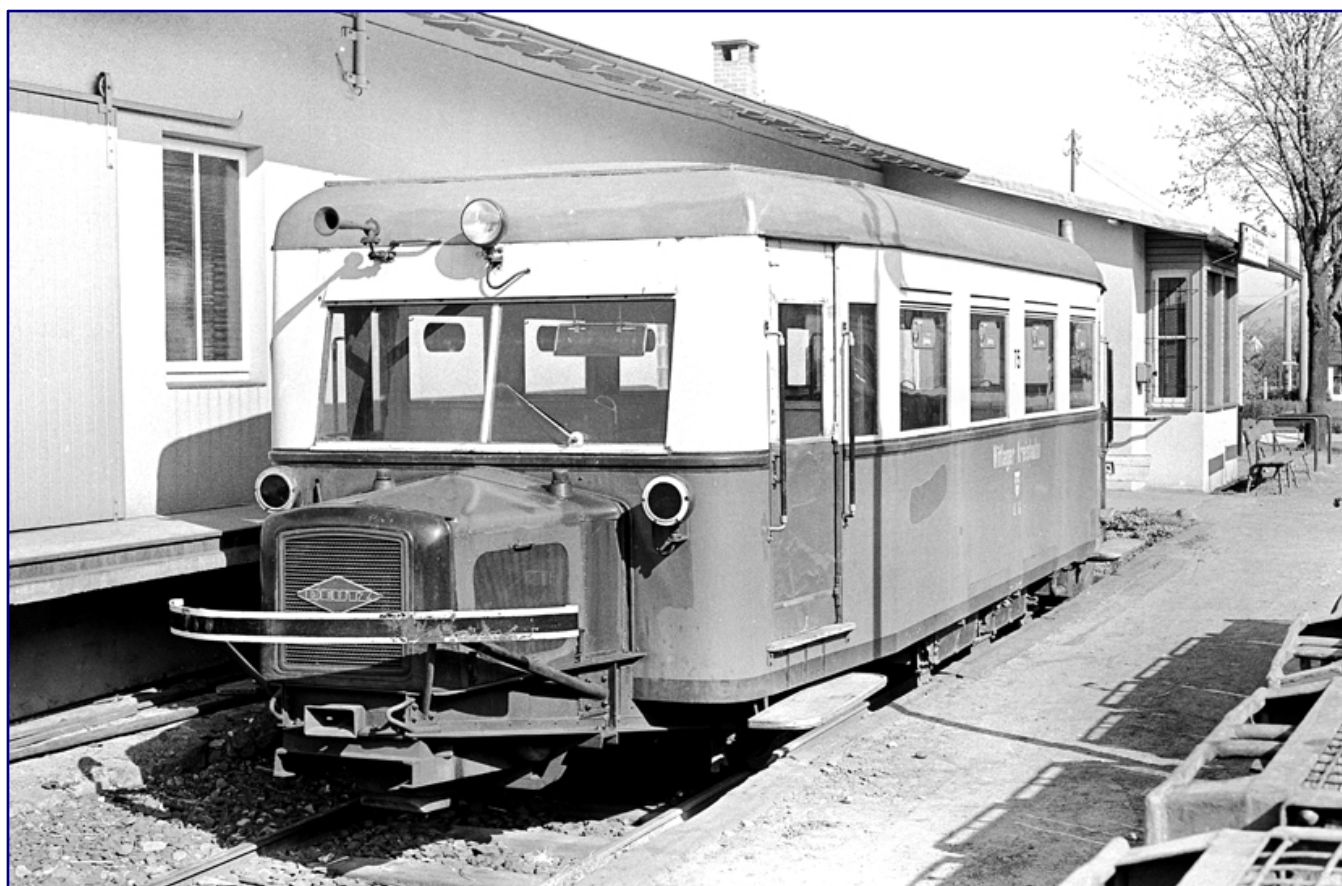
Wismar railbuses were not procured directly by the Deutsche Reichsbahn-Gesellschaft, but ended up in their stock in a roundabout way. This was also the case with the four type A vehicles, which were originally put into service as SAAR 73 to 76.

They were delivered to the railway division of the Saar region in 1934, but were taken over by the DRG as 135 077 to 135 080 as early as 1935. All four units survived the Second World War.

While 135 079 was already taken out of service in 1946, the other three were re-designated VT 88 900 to 902 in 1947 and were finally transferred to the Bundesbahn, which, however, took them out of service in 1950 as part of type reductions (VT 88 902: 21 December 1950) and offered them for sale.

In view of the lack of pictorial evidence, it therefore remained disputed whether VT 88 902 actually still had the DB's purple-red railcar livery, which Märklin also chose for the first edition in Z gauge. From Göppingen, it is said that the surviving logbook has a corresponding entry, from which the imprinted inspection data may also originate.

This longer type A had some special features compared to the other designs: They were longer both in terms of the wheelbase and over buffers, offered more space in the interior and, with a permissible speed of 60 km/h, outperformed their other family members.



Once again, we show the T 5 of the Wittlager Kreisbahn. On 29 April 1967, it is parked at Preußisch Oldendorf station. Built in 1934 at the Waggonfabrik Wismar, it entered service as 'SAAR 73' and was classified by the DRG the following year as VT 135 077. At DB, it was designated VT 88 900 for a short time. Photo: Gerd Wolff, Eisenbahnstiftung

And only here could both engines run simultaneously at full capacity and provide traction. Power was transmitted via a Mylius gearbox. Diesel engines were used right from the start. The braking system consisted of a foot brake and parking brake, which could also be used as an emergency brake.

A two-frequency typhoon (horn) emblazoned the ends of both driver's cabs. The original roof racks, which can also be seen on the model, were later removed and were in any case no longer present on the Wittlager Kreisbahn, as can be seen from the photos.

After being taken out of service by the DB, all three veterans were taken over by the Wittlager Kreisbahn as T 4 to T 6 and remained in service there until the mid-1960s. T 4 and T 6 were decommissioned and scrapped there in 1966, Märklin's original met this fate in 1972.

Only T 5 was sold to the Interessengemeinschaft Historischer Schienenverkehr (IHS) in 1970 and was passed on to the Noordnederlandsche Museumspoorbahn Assen-Rulde (Netherlands) in 1975. Just two years later, it was transferred to the CFV3V museum railway in Belgium, where it remained until 2014. It was then taken over by the Preußisch Oldendorfer Kleinbahn-Museum, which intends to restore it to working order.

A look at the model

Now that we have been able to shed some light on the history of the prototype, let's take a look at how Märklin tackled this challenge. Z-Modellbau had already presented the smaller Type B in 2014 and set new standards. This is why we once honoured this model as the new release of the year.

However, as this small series model is no longer available, there was potential on the market that Märklin wanted to utilise. This puts us in a position to draw the one or other comparison when testing the recently delivered novelty, released as Insider Club Model 2024 (item no. 88820).



Precise details make Märklin's new model (item no. 88820) look like a small series model. In fact, the model from Z-Modellbau is also used for comparison in places.

Straight out of the box, we hold a lightweight that weighs less than 15 grams and immediately makes an excellent impression on the outside. However, it also needs to be handled with care, as delicate attachment parts could otherwise be bent or even broken off.

Are we actually dealing with a small series model in this case? The comparatively low price does not lead one to expect so. In fact, the delicate, separately attached ladders to the roof look extremely filigree and are etched parts.



Attached grab rails, access ladders, and the filigree roof rack make the VT 88 902 look like a small series model.

But that's not all, because the roof rack and the adjacent gangways have also been manufactured and fitted in the same way. Extreme caution is required here, which many mass-production customers will certainly not expect.

This is why we explicitly point out that it is better to 'tip' the model out of the box into the palm of your hand than to grab it too roughly and pull it out. When we pick it up to rerail it, our fingers should not rest on the ladders!

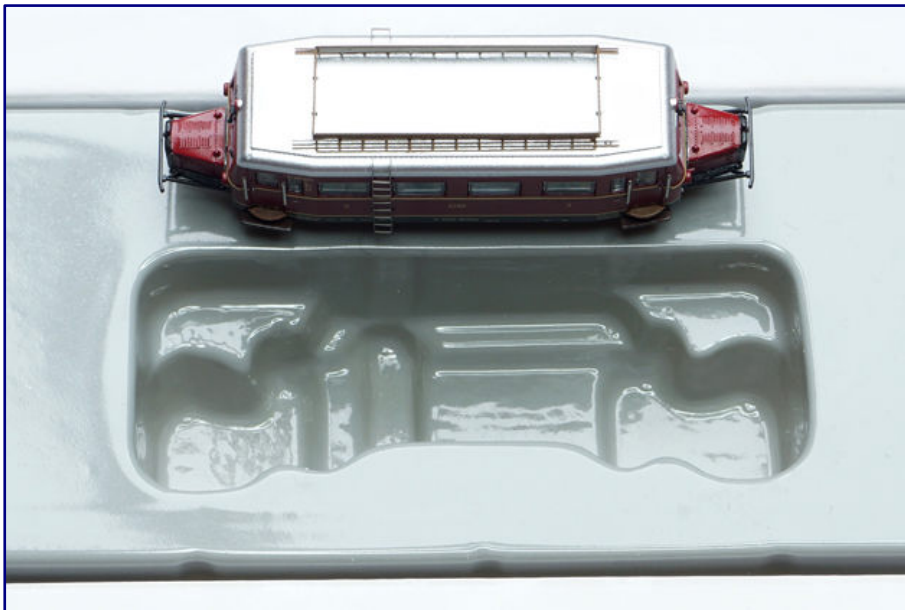
Care should also be taken when putting it back into the deep-drawn insert: The stepladder must now be on the left, because only on this side is there a recess in the box that can accommodate the opposite side, otherwise, it would be irreparably damaged. In addition, the Piggy's Snout must be inserted carefully and straight, so that no pressure is exerted.

If these guidelines are observed, the owner should enjoy the model for a long time and can truly savour a masterpiece of unusual finesse.

The running gear and engine bonnets of VT 88 902, designated as type Cvt 34, are made of metal. The rest of the body is moulded plastic.

The good detailing also includes other parts, including the correct reproduction of the buffer beams that are typical for the vehicle, which in this case are not fitted with carriers for bicycles and milk cans.

Separately attached are also handrails on all four entrance doors, which incidentally were pushed open on the prototype. Trafofuchs also offers an insert with suitcases for the luggage rack, which indicates a well-utilised railcar.



The thermoforming insert only has a recess on one side to accommodate the ladder: If the model is inserted the wrong way round, there is a risk of damage to this part!

Dimensions and data for the Cvt 34 (VT 88 902) of the DB:

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>	<u>Deviation</u>
Length over buffer (LüP)	11.700 mm	53,2 mm	52,9 mm	- 0,6 %
Length of the superstructure	11.240 mm	51,1 mm	51,8 mm	+ 1,4 %
Height above rail head *	3.159 mm	14,4 mm	15,4 mm	+ 6,9 %
Breite	2.902 mm	13,2 mm	13,4 mm	+ 1,5 %
Wheelbase	6.000 mm	27,3 mm	27,4 mm	+ 0,4 %
Wheel diameter	680 mm	3,1 mm	5,0 mm	+ 61,3 %
Service mass	10,1 t	---	12,5 g	
Axle formula	AA dm			
Performance	2 x 50 PS / 2 x 37 kW			
Permitted maximum speed	60 km/h			
Year of construction	1934			
Quantity	4 units			

* Dimension without luggage rack

In fact, it was probably rarely occupied, but when it was, it was with suitcases and crates, not bicycles.

However, as the vehicles travelled on less frequented routes, the small amount of luggage was probably often stored in the interior.

The good appearance is also underlined by the clean paintwork in the correct colours and the flawless printing.

Märklin has also used its printing skills to emphasise important features: We noticed the seals of the snouts in the transition area to the

wagon body. The radiators are also set off in black, which was not the case with the small series model.

As a result, the silver-coloured Deutz Diesel logo is also very striking. The two steps on the entrance doors are also highlighted in colour, the upper one in a lighter brown.

The three-light headlights and the two-frequency horn above the front windows at both ends of the vehicle are very distinctive. The operating markings can be read with a magnifying glass, but are not as sharp as on other models.



You can see a lot of lettering on the frame and car body, which is flawlessly printed, but not nearly as fine as on other Märklin models. This photo also clearly shows the sand pipe leading to the first axle. On closer inspection, the four drill holes for the stepladder attachment show that they are too large.

We can read that the last inspections took place in 1948 and 1949, the vehicle weighs 10 tonnes and has a wheelbase of 6.00 metres. The brake system is labelled Knorr-Bremse. 50 passengers can be seated in the railbus, which runs on roller bearings. The labelled maximum speed of 60 km/h of a railbus assigned to Nürnberg (Nuremberg) for maintenance also corresponds to the prototype data.

There is no doubt that this is an extremely fine model, but unfortunately it also has a few weaknesses. For example, the slightly too large holes in the side walls for the stepladders are distracting, at least in some of the photos. However, in contrast to the fine rivet replicas on the roof edges, this is not noticeable without a magnifying glass.

However, although almost all the prototype dimensions are accurate to scale, the 'Anteater' appears unusually and disturbingly long-legged when viewed from the side. In fact, there is a measurable deviation of a full millimetre in height.

This is due to the drive wheels being far too large: they are almost two millimeters or almost two thirds larger than the scale conversion! A comparison with the model from Z-Modellbau, which is based on identical prototype dimensions at this point, confirms the enormous outlier.

We accept it, because Märklin was able to explain it. The focus here was on operational reliability, as the miniature was not supposed to get stuck on turnouts or have problems with switching tracks. However, as the latter are not installed in our oval test track, we are unable to verify its success.

Let us therefore conclude our look at the running gear with a word of praise. In addition to the fine replicas of the bearings and other features, we were particularly impressed by the sand pipes, which we were not expecting. Only a look from below surprisingly revealed that they are not at wheel level.

The interior also comes off well in the test: With such a small model, there is not much room for details. Märklin has used what is not occupied by the bell-type armature motor for an implied interior, which is visible through the crystal-clear windows.



A comparison with the small series model from Z-Modellbau proves a comparable filigree quality. At the same time, however, this photo also exposes the much too large drive wheels, which are due to the operational safety on all Märklin track.

For example, we discover indicated seat backs in the interior and a driver's console in the centre of each end, which has a prototypical wood tone. Overall, the new product is therefore completely suitable for display cabinets, and, thanks to the usual perspective, the oversized wheels will probably not be noticeable on the layout.

Technology and conclusion

However, a Märklin model should not only please collectors and lead a showcase existence. That is why this model for early Era III also has to prove its driving skills on the test track.

We know from the likewise two-axle railbus (VT 98) that such a vehicle with two sliding contacts per pole quickly reaches its limits.

In fact, the latest-generation high-efficiency motor is probably not supported by a flywheel and cannot bridge short unpowered sections with capacitors. In view of its (too) low weight, this quickly becomes its undoing.



At the front, the Deutz diesel logo is clearly visible on the radiator grill, which is clearly separated from the body. Above the front windows, we recognise the two-tone horn, surrounded by the three LED-lit lanterns for the head and tail lights.

We are finding this tiny thing to be much more sensitive to voltage than we expected. For example, we had to carry out some measurements several times during the test, because even short voltage interruptions caused a standstill.

We see potential for optimisation in the following areas: larger contact surfaces on the inside of the wheel discs, flywheel mass or power buffering and, in any case, an attempt to increase the dead weight. Even the somewhat smaller Type B from Z-Modellbau exceeds its weight on the precision scales by almost 2 grams.



The distortion-free side view provides a clear look into the passenger compartment thanks to the crystal-clear windows. Only a comparatively small elevation was required for the engine in the cover section.

In contrast to this, however, Märklin's realisation is equipped with direction-dependent light: warm white three-light headlights and two red taillights, generated by maintenance-free, light-emitting diodes. These are also used for the interior lighting on the circuit board installed in the roof.

Unfortunately, they are only visible when the model is already running on some speed: The head and taillights operate at around 4.2 volts, while the interior is only slightly illuminated at around 5.0 volts. The little Piggy's Snout is already running at 2.3 volts with a converted speed of 13.8 km/h. Even the lowest continuous travelling speed of 22 km/h at 3.2 volts of direct track voltage still happens without any light at all.

The prototype speed of 60 km/h is reached by the new model at 5.2 volts track voltage, i.e., when the interior lighting first appears. If we apply a maximum rated voltage of 10 volts to the track, at least the model can compete with modern vehicles: its maximum speed measures 120.4 km/h.

This is not in line with the NEM specifications, but does not bother us because the adjustability is good overall, and the prototype speed is clearly too slow by comparison. In addition, the risk of stoppages is then also significantly increased. Due to the lack of an effective flywheel mass, the miniature comes at prototypical speed to a standstill after only 13 mm when it reaches a powerless section.

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The model cuts the best figure from a slightly elevated view (photo above), because the large wheel discs then recede visually. The separating edge between the bonnet and the body is also clearly visible. From the passenger's perspective in a slightly angled view, on the other hand, we finally notice the seat backrests (photo below), which were described in the article as a feature of the implied interior decoration.

The current consumption of 9.4 mA at transformer position 100, 13.5 mA at 150, and 51 mA under full load is within the expected range. We did not measure the tractive force because the conversion has no coupler, and the prototype was also travelling without a trailer car.

This brings us to the point where we want to draw an overall conclusion from all the observations and measurements. This is very positive overall, but must not leave out points of criticism that the manufacturer should address.

Above all, the sensitivity to electrical contact is too high, in our opinion. While this model may perform well under laboratory conditions without dust, it may drive the owner crazy during trade fairs, where dust and other floating particles fall on the system for many hours.



Surrounded by contemporary car models (by Pánits László), the Wismar rail bus cuts a fine figure in layout service. It always requires absolutely clean rails, otherwise interruptions to the journey are inevitable.

At home, it demands very high standards of absolutely clean rails if interruptions and dropouts are to be avoided. Even during our test, the running gear and electrical contacts had to be cleaned several times, and not all measurements were possible straight away. This is a bit too much for us.

Visually, the model is on a par with expensive small series models, but costs only a fraction of the price. If the owner is careful when handling the model, he will get a lot of value for his money. If we also consider the fine printing and the lighting equipment, the gap widens even further.

Only the high legs due to the clearly too large wheels are a setback, but can be forgiven due to the declared focus on operational suitability. For this very reason, however, the electrical contact reliability should be significantly increased, otherwise this argument will come to nothing.

Taking all factors into account, this realisation deserves to be nominated as one of the best new product releases of 2025 in the locomotives category. We are curious to see which models the Wismar railbus will have to compete against in the end and how it will perform.

Manufacturer of the model:
<https://www.maerklin.de>

Loading for the roof:
<http://www.trafofuchs.de>

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Current deliveries

Too good to forget

After a rather slow start, Märklin has delivered a lot of new items in the last few weeks. We see the danger that some particularly successful vehicles are in danger of being lost in the maelstrom of speed. Some of them have the character of mould new products, which is why they should be presented in more detail here.

The most recently delivered models include the class E 1012 electric locomotive in cobalt blue-beige colour scheme (item no. 88415). The version, known as the “crease”, was previously missing from the programme several times and was, therefore, warmly welcomed.

Unsurprisingly, it comes with direction-dependent headlights generated by warm white LEDs, a bell-shaped armature motor, and a roof that no longer has a hole for the changeover screw. It is therefore technically up to date and has undergone a product upgrade.



This is not the first time Märklin has been inspired by the Rheingold. The individually available locomotive (item no. 88415) can also enrich earlier packs. Photo: Michael Böttcher

One of the machines introduced in 1962 with the characteristic, streamlined front, which had led to the well-known nickname, was selected for the return. These locomotives from the first series were intended for the long-distance express train “Rheingold”, which was launched in the same year with new rolling stock and was the Bundesbahn’s top product – even above the TEE comfort train.

It may therefore come as no surprise that Märklin has once again thought of precisely this train. After all, the “Knick” in this colour scheme was still missing from the programme: In 1998, a box E 10 was issued with the train set 81412, thus reproducing a provisional version of the prototype that had been in service for a few months.



It looks good, the new crease (88415). But unfortunately, the chosen road number does not match the Märklin design. Writing the correct number would not have caused any additional costs: an avoidable mistake!

Märklin also had to improvise here because there was not yet a crease (and incidentally also no hump dining car) in the mould construction kit. This was different in 2007 when the sister train 'Rheinpfail' followed under item number 81439, which exchanged course cars with the 'Rheingold' in Duisburg Hbf.

The train was finally complete as far as the liveries of the carriages and the locomotive were concerned. Somewhat unusual for many customers, the train appeared in a mixed colour variation of cobalt blue-beige and purple-red-beige. However, after its upgrade to the TEE category in the transition period of 1965/66, this was completely in keeping with the original colour scheme.

The chosen road number of the E 10 1309 also matched the shape almost perfectly. Unfortunately, this does not apply to the latest edition. Märklin chose E 10 1266, a model from the first series of Rheingold locomotives, which, however, did not have the all-round holding bar at the front. We prove this with the two prototype photos.

We had previously pointed out this avoidable mistake, so it was probably made deliberately. If, for example, the road number E 10 1312 had been used, this would also have fitted, especially as the replicas built for the "Rheinpfail" also pulled the "Rheingold". We therefore do not regard it as a compromise.

Incidentally, E 10 1312 still wore its original colour until 1966 and could therefore be used quite widely. E 10 1308, 1310, and 1311 would also have been suitable alternative numbers, as the road number 1309 was already a template.

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The real E 10 1266 is already wearing purple and beige during the exchange of coaches between TEE 9/10 "Rheingold" and TEE 21/22 "Rheinpfad" on 26 February 1968 in Duisburg main station (photo above). The front, which is not identical to the Märklin model, is nevertheless striking. Photo: K.D. Hensel, Eisenbahnstiftung

E 10 1312 still wears its original livery in Frankfurt (Main) main station in 1965 (photo below). With its surrounding handle bar and tread plates, it would have been a suitable model to deliver a correct model without additional effort. Photo: Reinhold Palm, Eisenbahnstiftung

A mistake has also been made again in the 87266 train set: The observation car has four side windows in the observation cab next to each other and is, therefore, one of the replicas for the Rheinpfeil. Below the window band, it would then be labelled "DEUTSCHE BUNDESBAHN" instead of "RHEINGOLD", but it was also used in both trains.

This is also not the first time that Märklin has ignored this, although there is unlikely to be a heart attached to the lettering. We would have expected the window bars, which are modelled in relief, plus the missing window bars for eight windows to be printed in a row, in order to get closer to the prototype. Unfortunately, this did not happen.

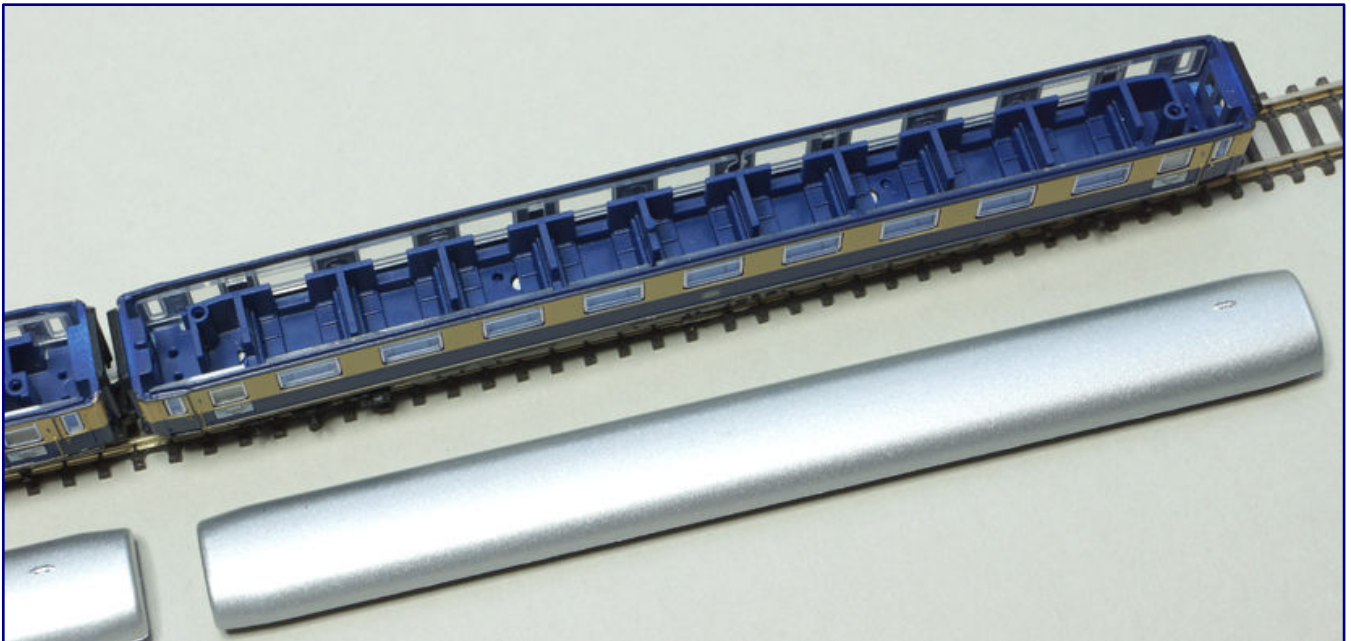


The Rheingold is divided into two car sets: The first (87266) contains two Av4üm-62 compartment coaches and the AD4üm-62 observation coach (photo above). The latter has always had an interior in the upper deck, which consistently follows the prototype and has therefore been sprayed red (photo below). The model still has to make do without such replicas on the lower deck.

However, the interior fittings, which were not announced and are still not to be found in the product description, deserve great praise! Only the AD4üm-62 observation car has always had them in the upper deck, and, in the bright red colour of the prototype, they are immediately noticeable. In this case, however, it is still missing in the lower level, where the bar and luggage compartment were located.

However, this is different for the two Av4üm-62 compartment coaches, whose seats were covered in blue. Märklin has given them new interiors with compartment walls and seats, which are definitely noticeable from the outside thanks to their strong colour.

This is also the case with the same example of car set 87284, where the same equipment can be found. The large-capacity coach Ap4üm-62 shows it in the same colour and similar shape, but naturally there are no walls in the interior here.



The second package (87284) provides, in addition to another compartment car, the Ap4üm-62 large-capacity car and the striking WR4üm-62 hump dining car (photo above). The interior of the compartment car reproduces the blue seats of the prototype (photo below).

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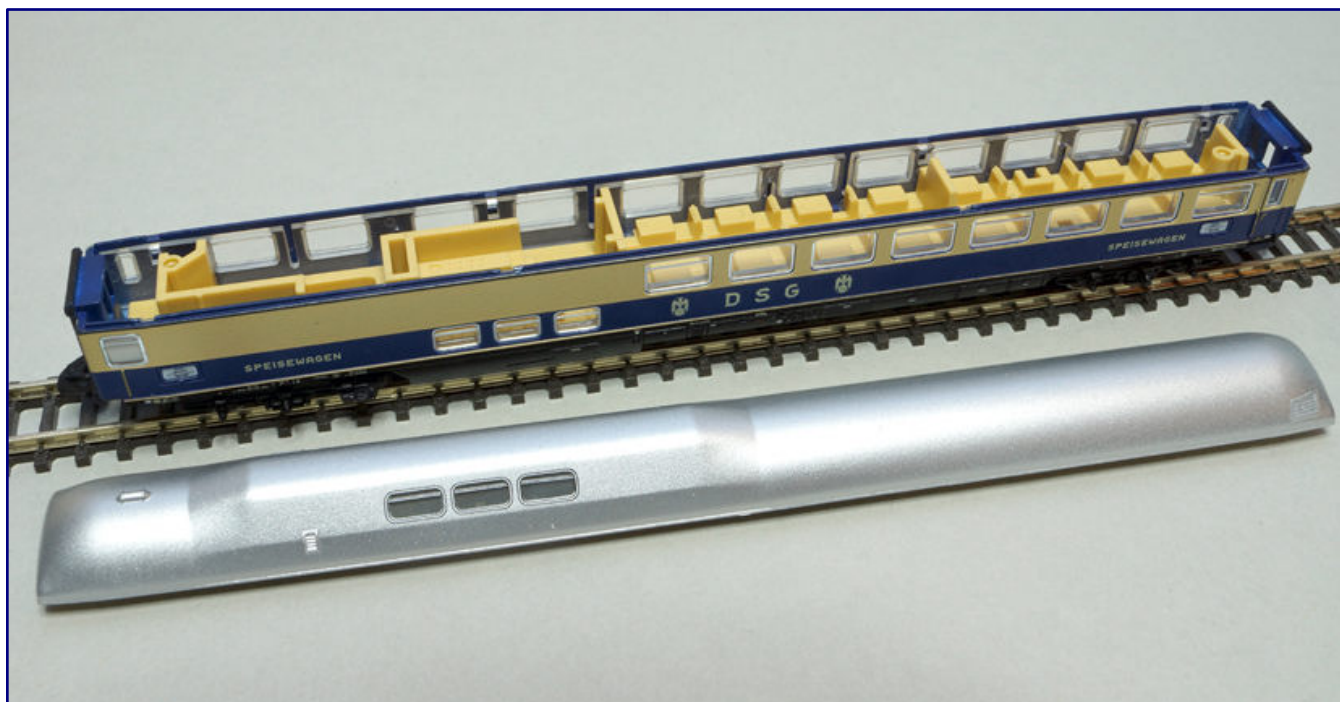
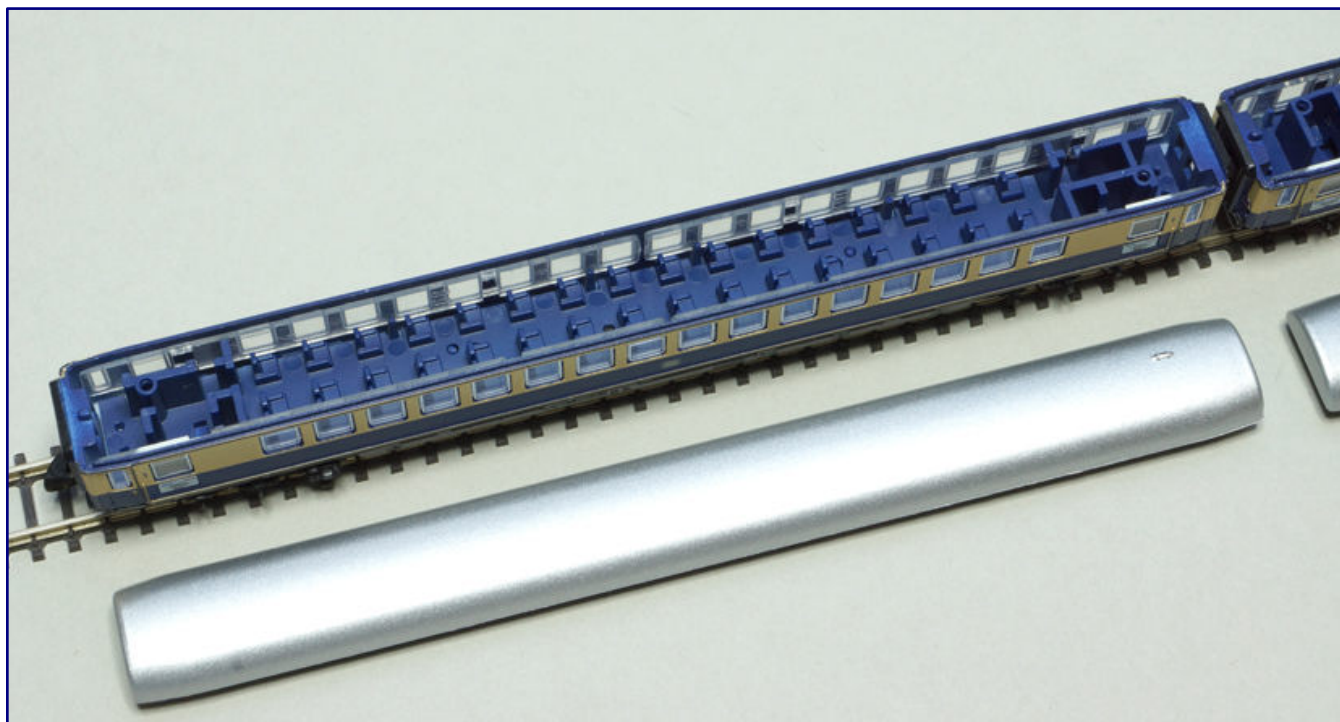


Photo above:

The seats of the large-capacity coach were also originally covered in blue on the prototype. Accordingly, the interior of the model car is identical in colour to that of the compartment car. According to the original, however, it shows a 2 + 1 arrangement with a centre aisle and toilet rooms at both ends of the vehicle.

Photo below:

The light-coloured interior of the dining car provides a visual contrast. In addition to two dining areas, separated by a centre wall, it also includes the double-decker kitchen, which was responsible for the distinctive hump in the roof of this carriage and created a unique selling point among all DSG dining carriages.



Do you recognise a difference between the windows in the lower and upper floors of the humpback dining car in the photo above? The lower windows are inserted separately, Märklin has made the upper windows appear more three-dimensional with imprints. The design of the car ends is also very pleasing to the eye (photo below): Both the cobalt blue and the beige colours run around the corners; the train end lanterns are set off in red. The elegant black decorative strip between the window strip and roof edge has not been forgotten either.

The WR4üm-62 humpback dining car has also become a delight. This is partly due to its new interior in a bright yellow colour, which includes the dining area and two-storey kitchen. Another plus point is the strikingly successful printing on the roof: this has been injection-moulded in transparent plastic and painted close to RAL 9006 white aluminium.

This was also the case before. The windows of the upper kitchen area were always omitted. For the first time, Märklin has now also elaborately printed the window frames and thus achieved that they are plastically set off and look like inset parts. It is a small step with a big effect.

Coating and printing are the consistent strengths of all three products presented so far: correct colours, a clean spray pattern and flawless, accurate pad printing. We were also pleased to note that the cobalt blue and beige-coloured stripes run cleanly around the corners of the wagon.

The transition areas are painted cobalt blue and all of the cars are equipped with close couplers. All in all, this product enhancement means that we are probably looking at the best edition of this train and it relegates all its predecessors to the sidings.

But there is another new product that deserves an equally close look: the class 038 (88909) steam locomotive with a tub tender. The Zetties were delighted with this second model after the museum locomotive 38 3199, which is again produced using the bronze injection moulding process.



Like its predecessor, which was also produced in bronze injection casting, 038 958-5 (88909) is also a three-domed representative of this series. The stencil-painted beams on the tub tender are attractively coloured red.

This is also a prototype with three domes on the long boiler. In contrast to the museum locomotive, however, we discover a driver's cab with a fan attachment over the full length of the roof. It also has a one-sided Indusi on the driver's side.

However, the magnet under the driver's cab and its holder as well as the connections have been realised more roughly than on the reference model. The elevation on the side wall of the driver's cab has a restrictive effect on the appearance, but it is prototypical: The Bundesbahn accommodated the electrical system there.



The reproductions of the box attached to the driver's cab and the Indusi locomotive magnet on the driver's side are striking (photo above). For the first time, Witte plates on the class 38 now have the correct size and also prototypical brackets (photo below).

It is correctly reproduced that the Ege biscuit and the locomotive number were attached to this box. However, in the prototype, the sign with the operating number protruded to the right and left, which also made the box look like a foreign object.

Märklin obviously did not dare to apply a separate plate here and simply scaled its pad printing to make do with the surface of the Indusi box. This is only a compromise, but we can accept it because the smaller font size is not too noticeable, and it avoids a part that is in danger of breaking off.

The running gear with its detailed control is also pleasing to the eye. The fiery red colour of the wheel spiders is so well done that there is no perceptible deviation from the circulation or the buffer beam. This is also due to the undetectable plastic lustre, which makes these parts look as if they have been painted separately.

The paintwork is clean over the entire model and even the operating inscriptions are complete and finely applied. At this point, we would like to emphasise the supports for buffers and coupling irons on the tub tender set off with red paint, as well as the flash warning arrows on the domes and inspection markings on the front buffer beam.



There are no differences in the degree of gloss between the body and chassis. We would also like to praise the side prints on the domes. And we are pleased to see that the new correctly sized wind deflectors no longer protrude over the buffer beam.

Märklin justifies the higher purchase price of an injection moulding model with extremely conscientious realisation. It is only a side note that the buffer sleeves are not also painted red. If you had the right paint to hand, you could fix this yourself and perfect the impression. The indicated brake hoses and coupling hooks could then also be painted black.

The special treat, however, are the now correctly sized Witte wind deflectors with equally correct brackets. All steam locomotive models produced in Göppingen otherwise have incorrect brackets on the Witte baffles. On the former P 8, they had to be made smaller due to its compact design and are roughly flush with the buffer beam at the front.

This was not previously the case with any of the models and therefore seemed somewhat strange. The effect of the modified part, which is part of the injection moulding and not moulded separately, is astonishing and very appealing.

We are therefore surprised that we have not yet heard any comments from readers. After all, this is probably the best model of the 38 series from Märklin, to date. If you have overlooked it, then take a closer look and compare the different models!

Manufacturer of the models:
<https://www.maerklin.de>

The path to the first layout (part 7)

Winter's sudden Return

With the last progress report on our snow themed layout dating back to January, winter is set to return once again today. Some of the preparatory work we made is now being finalised and we set the stage for the next phase of the build. And then there is a model that we want to build largely from scratch, for lack of any commercial alternative to suit our needs.

To finish off the work from the last episode on our still snowy layout, we equipped the village houses and the railway station building with icicles from the Faller range to get them ready for winter.

Later, their roofs will also receive a snow cover matching their respective pitch and insulation underneath the roof tiles. We have already procured the necessary material for this: Busch offers a crystal snow paste (item no. 7172), which should be ideal for this purpose.



Today we are finally moving on with the winter layout. This time it's mainly about building an additional structure, but not a standard one straight from a kit.

The powder snow from Noch (08750) will also be used and the landscape will look as if it has been covered in powdered sugar. But before we get to that point, we still have a few details to prepare, which, for us, are simply part of the process.

We had already adapted a deciduous tree armature from Faller for our purposes and took some accessories from the “village decoration” pack, also from Faller. In the meantime, we have covered all pieces with grey spray paint to take away the plastic shine. This should be enough for the monument, but the bench and the tree will change from grey to a matching brown.



After determining the tree's position in the last episode, it was now time to colour the selected parts: Priming was followed by a brush application of quick-drying Tamiya colours, which are ideal for painting plastic parts.

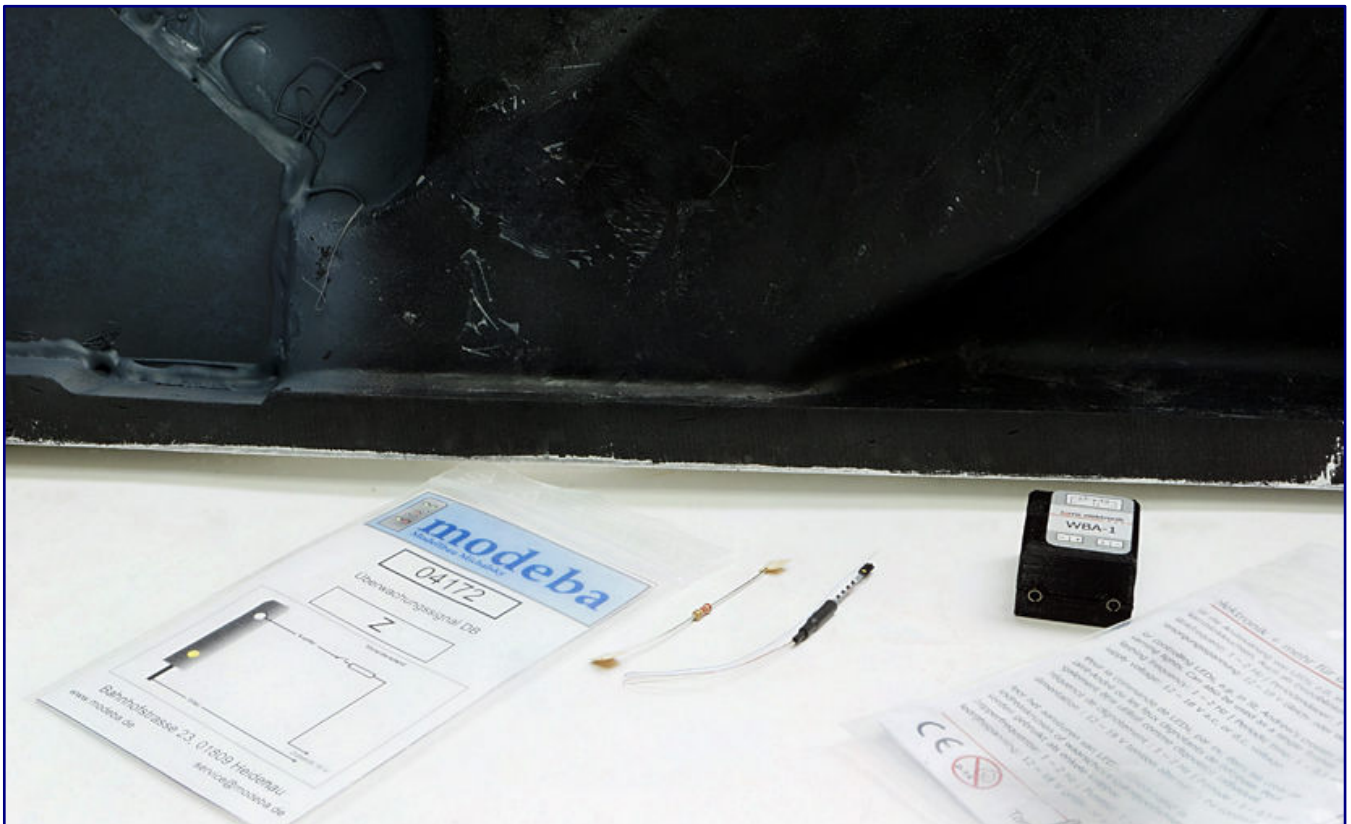
The trunk and branches of the bare deciduous tree receive a brush coating with Tamiya's red-brown colour (XF-64) to give them a prototypical matt finish. The round bench is given a lighter colour (XF-52; “Earth matt”) to make it stand out a little better.

Incidentally, the tree that has now been prepared, apart from the obligatory snow tracks, will not be the only one on the small layout, but will probably remain in the village area. The slopes in the background will later be planted with Busch snow fir trees (6566). Each pack contains 20 of these, which will not be nearly enough.

We need two to three packs even for such a small layout. It is therefore best to procure them early: First, trees are a financially large item for any layout, and, second, this way there is still time to back-order them from the dealer in case of insufficient stock.

In the area of one of the two farms, we also foresee to place the model of a draw well which we already had in our collection. It's a little gem of a model which came as a give-away sample from Märklin two years ago to showcase their bronze investment casting production technique. It will therefore also be prepared with a grey spray primer and then be brush painted with various Revell paints.

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The bare tree, round bench, and monument after being painted (photo above). In the area of the small park, we also plan to install a billboard from HOS-Modellbahntechnik, as well as a railway telephone from the same company. The level crossing monitoring signal has been supplied by Modeba (photo below), and we still must find a space under the layout for the flashing electronics from Tams.

We are also preparing to add further details to the layout: we have procured a ticket machine (2088-5) matching the period depicted and an illuminated Sinalco drinks machine (3141-5) from the Modellland programme. A cut-out sheet with advertising posters (7530-5) will also be of great use.

We also chose a trackside telephone and an advertising billboard from HOS-Modellbahntechnik, which are listed in the 1zu220-shop range. We will put up the billboard later in the small park behind the railway station.

A Modeba level crossing monitoring signal (04172) will later be installed in the clearly visible area of the inner track circle. This still needs electronics to make the white LED in the signalling screen flash at the appropriate intervals.

At the same time, it must be designed in such a way that it can be permanently installed in the hollow spaces underneath the layout. We found the right product at Tams Elektronik: Alternating flasher WBA-1 (53-03017-01) with adjustable 1 to 2 Hz flashing frequency and fixed housing to protect the component.



Testing the position of two shelters for agricultural equipment (item no. Z101B) from Klingenhöfer Miniatures: Their overall length is too great for leaving any space for the farmhouse which we also want to place in this corner. At the same time, their width and height are not sufficient to accommodate a modern tractor. In other words, we must find an alternative solution!

Today, however, all these parts are only being tested for fit and installation. Construction is to take place elsewhere. There is a farm on the right at the front edge of the layout, which is to be extended next to the residential building. An agricultural shelter for vehicles and equipment is planned here.

This should be clearly visible to the viewer from the front, because this is the spot where we want to display a Mercedes tractor "MB trac" with minor defects, which will be preserved as a collector's item and partially covered with a tarpaulin.

We roughly determine the required basic dimensions with the help of the tractor model and initially decide in favour of a complete construction made of wood. Fortunately, however, we came across two shelters for agricultural equipment (Z101B) from Klingenhöfer Miniaturen; they correspond completely to our idea and are made of milled polystyrene.

However, as they are too small to accommodate more modern and therefore larger tractors, we have come up with a way of modifying them. Unfortunately, combining them into one is not enough. We will not only have to dismantle them, but also extend them in all directions.

We therefore carefully cut off the roofs with our Faller craft knife. We remove two stand widths of the rear wall from the still completely unpainted model. Then we also cut open the rear wall of the other model and add this piece there.

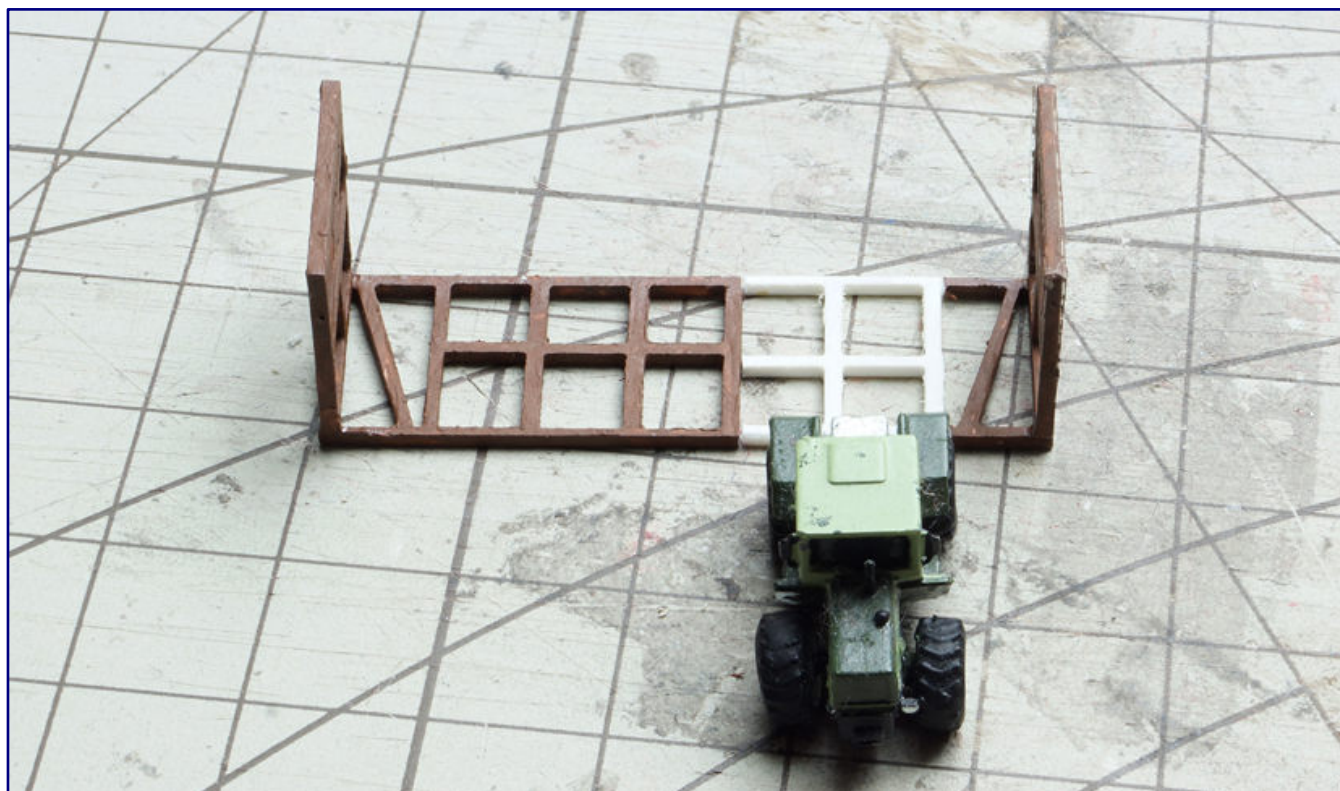
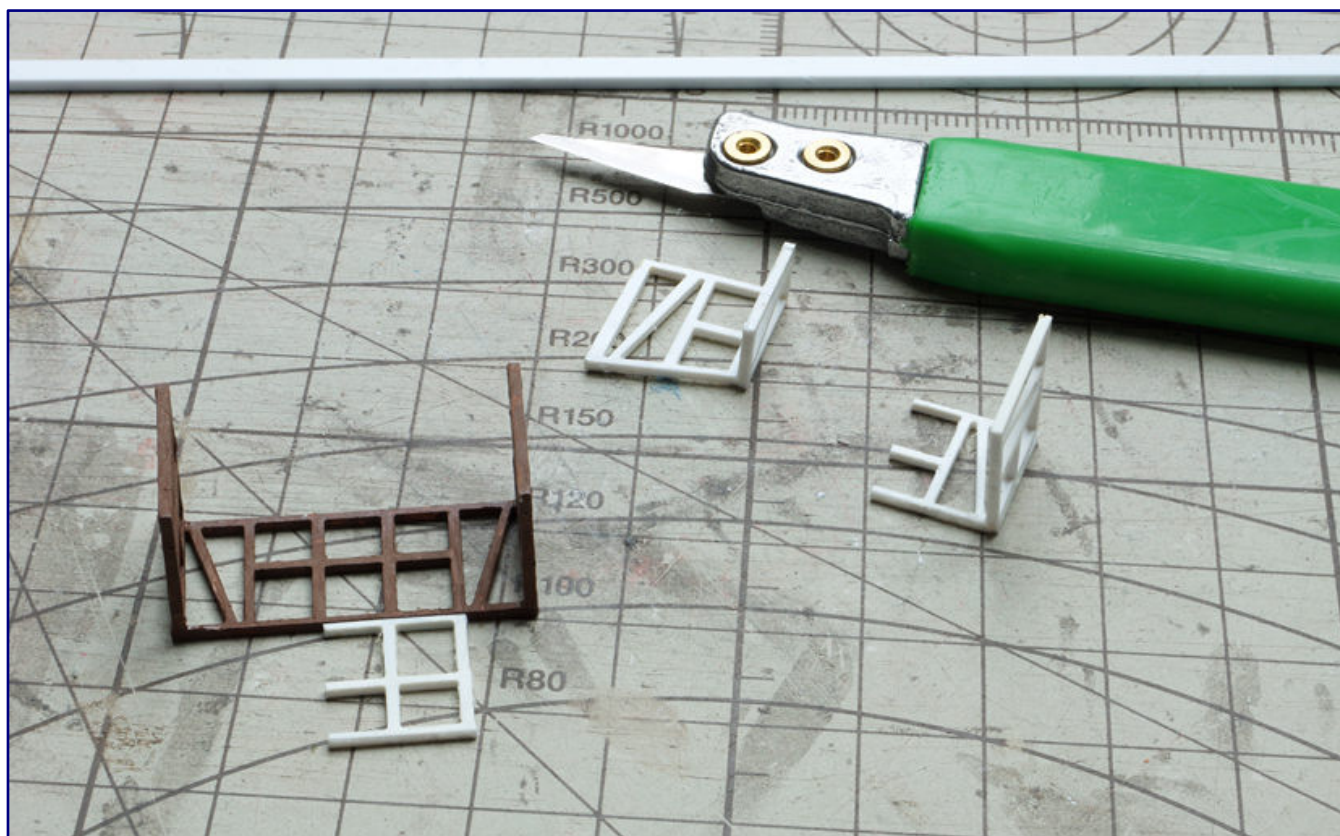


Turning two into one! Our kit bashing begins with cutting off the roof support. Then, we will also dismantle the frame.

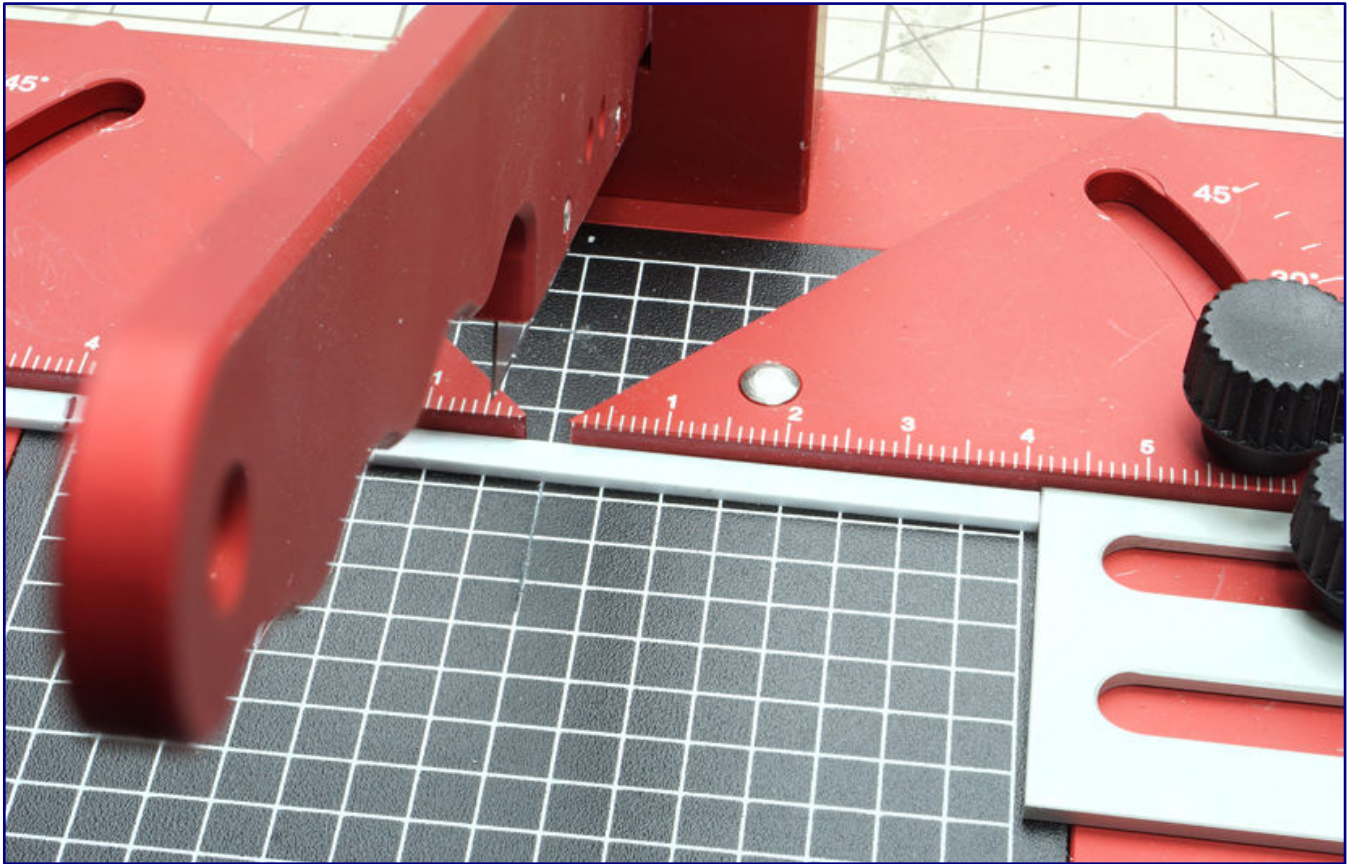
Pattex Special Modellbau glue ensures a secure adhesive bond, which can be dispensed finely and precisely with its needle. As it also sets quickly, we make rapid progress. Now, we measure the length of the rear wall (4.2 cm) and cut a plinth from Evergreen polystyrene strips (rectangular profile) to raise the wooden framework. Later, this will replicate a concrete foundation.

Cut two more of these strips to the same length (2.5 cm) and use them to extend the foundation towards the front. The "Mitre Cutter Tool" (RP-CUTR) from RP Toolz is a useful tool for this work, as it allows to precisely preset the cutting length and also execute mitre cuts – this will become important in a moment.

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We cut out the width of two frame segments on the long side from the frame of the still unpainted shelter (photo above). We then re-insert them in the appropriate place in the cut-up uprights of the other shelter (photo below). After gluing, we continue with the construction of our open vehicle shed.



With the help of the RP Toolz mitre cutter (RP-CUTR), the polystyrene profiles for the concrete base of the vehicle shelter can be cut to length quickly and precisely (photo above). The mitre cuts for all sloping beams in the roof area and stud frame are also perfectly executed with this tool (photo below).

Using the same procedure, we also add polystyrene strips (square profile 1.5 x 1.5 mm) on the upper side. Let them protrude slightly beyond the base at the front so that they can then be cut to the right length.

After applying the adhesive, place the respective side panel on the cutting mat and place the steel ruler on it so that it marks the cut length at the right angle in the roof area. With the craft knife, it is now a small cut and the part fits. Repeat on the opposite side.



A small preview of the following parts: It was already mentioned at the beginning that we are procuring our material before or early on during the build. This also includes a ticket and drinks machine from Modelland, as well as (historical) advertising posters that have survived for many years on some forgotten building walls.

We now also prepare the two vertical end supports; the angle of the roof pitch must be observed in the upper area. Finally, the RP-Toolz tool also provides us with the right moulding to glue between the beams of the slopes at the front edge of the roof. We have now built a roof ring anchor, which would make the building more windproof.

The adjustment options of the cutting beam now allow us to cut a support to be installed in the centre at the front, including the concrete base, as well as two short diagonal struts for guiding the forces. The new stud frame on the side walls also requires compartments and diagonal braces to transfer the forces from the roof to the ground.

Without changing the character of the Klingenhöfer shelter, we have now enlarged the building to the required extent. We carry out a final test on the layout and are satisfied. The pantile roof and the wooden panelling on the walls are still missing, which will be added in a subsequent episode.

We will also prepare the colouring of the still white plastic material by then. A layer of matt brown acrylic spray from Noch (61173) quickly makes for the impression of sturdy wooden beams, which will only be visible on the inside after panelling. We also use the same acrylic paint to remove the plastic lustre from a Kibri hunter's fence from our box of spare parts. It will secure a section of the property next to the shelter to the railway line.



Our customised vehicle shed is visible to the observer, and there is now also enough space for the farmhouse. We will enclose the property at the rear, but first we will give the plastic parts a matt brown coat of acrylic spray from Noch. And soon the roofers can get to work!

In the next episode on building this beginners' layout, the landscape will take on more shape, as some additional parts will be installed. We are excited to see how they will all look together. And then we'll have to let it snow soon...

Material used:

<https://www.faller.de>
<https://www.klingenhoefer.com>
<https://www.noch.de>

<https://www.pattex.de>
<https://www.rptoolz.com>
<https://www.tamiya.de>

Vorbereitende Einkäufe:

<https://www.busch-modell.de>
<https://www.modeba.de>
<https://www.modellland.de>

<https://tams-online.de>
<https://www.1zu220-shop.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Rheinische Eisenbahngesellschaft **Untergegangener Gigant**

Die Rheinische Eisenbahngesellschaft war eine des großen Bahnunternehmen aus der Pionierzeit der Schienenwege. Sie erschloss den Aachener Raum und das Rheintal, um dann als dritte Gesellschaft auch von der Wirtschaft im Ruhrgebiet zu profitieren. Wie ihre Konkurrenten erlebte sie große Renditen und herbe Verluste, bevor Preußen die Eisenbahn zur Staatsaufgabe machte.

Bernd Franco Hoffmann
Die Rheinische Eisenbahngesellschaft
Geschichte und Gegenwart in faszinierenden Bildern

Sutton Verlag GmbH
Tübingen 2024

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Bernd Franco Hoffmann arbeitete als Autor und Journalist u. a. für die Kölnische Rundschau, den Kölner Stadt-Anzeiger und die TAZ sowie für Sender wie WDR, RTL und Kabel 1.

Aus seiner fachkundigen Feder haben wir bereits die regionalgeschichtlichen Bände zur Köln-Mindener Eisenbahn und der Bergisch-Märkischen Eisenbahn vorgestellt.



Diese beiden Gesellschaften erreichten als erste und zweite das Ruhrgebiet, um so den Grundstein für den rasanten Aufstieg des Kohlebergbaus und Stahlproduktion zu legen. Die Rheinische Eisenbahngesellschaft war das dritte große Unternehmen im heutigen Nordrhein-Westfalen, das schließlich auch ins Ruhrgebiet drängte.

So taucht der Autor tief in die Geschichte ein und führt aus, wie wenig sich Preußen nach Errichten der ersten deutschen Eisenbahn um das neue Verkehrsmittel kümmerte, seine Möglichkeiten verkannte und den Bau von Eisenbahnstrecken auch nicht förderte.

Damit war der Start in der preußischen Rheinprovinz (wie auch in Westfalen) keineswegs einfach. Impulse für die Eisenbahn kamen von Friedrich List sowie den Gewerbetreibenden landein und landab, die nur erfolgreich sein konnten, wenn sie ihre Erzeugnisse schneller und günstig auch in weiter entfernte Märkte schaffen konnten.

Nur privates Kapital kam in Frage, um solche Vorhaben zu finanzieren, Städte und Gemeinden förderten dies, um einen Anschluss an die weite Welt zu erhalten und langsam, dann aber immer schneller, entstanden einzelne Strecken und kleinere Netze, die schließlich auch zusammenwuchsen.

Doch bis dahin war es stets einer weiter Weg, der immer wiederkehrende Prozesse auslöste: die Suche nach der besten Trassierung, Prüfen der Machbarkeit, Erlangen einer Konzession und Einsammeln des erforderlichen Kapitals, um mit dem Bau beginnen zu können.

Bernd Franco Hoffmann skizziert ausführlich, wie die Rheinische Eisenbahngesellschaft (RE) entstand und durch Höhen und Tiefen gehen musste. Auch ihr Führungspersonal änderte sich, zeigte aber schließlich eine beeindruckende Konstanz.

Geprägt ist ihre Geschichte bis zuletzt von Konkurrenz und dem Mitprofitieren renditeversprechender Verbindungen. Das brachte den Bahngesellschaften, die sich zwangsläufig auch untereinander nicht grün waren, häufig auch laute, öffentliche Kritik ein. Widerstand kam lange und oft vom Militär.

Wir tauchen tief ein in die Geschichte des 19. Jahrhunderts, um den Zeitgeist aufzusaugen und zu erleben, wie sich die politischen Ansichten zu ändern beginnen. Schließlich beteiligt sich auch der preußische Staat an Bahnunternehmen, häufig zum Abwenden existenzbedrohender, finanzieller Schieflagen. Inzwischen ist die Eisenbahn unverzichtbar geworden.

Verfolgen können wir, wie die Rheinische Eisenbahn den Aachener Raum erschließt und einen Anschluss nach Belgien schafft, sich in der Eifel schwertut, im mittleren Rheintal baut und so den Süden mit dem Norden verbindet.

Schließlich sollen der Raum Düsseldorf und Wuppertal selbst erschlossen werden, um bis ins Ruhrgebiet zu gelangen, wo Kohle und Stahl satte Gewinne versprechen. Auch viele Fehlschläge sind darunter und drei konkurrierende Bahnunternehmen verkrachtet auch das Ruhrgebiet nicht auf Dauer.

Schließlich erleidet die RE dasselbe Schicksal wie ihre Konkurrenten. Der Staat schafft nicht nur eine Verwaltung für die Bahn und mischt sich ins Geschäft ein, sondern beteiligt sich an ihnen, kauft sie auf und führt sie schließlich in einer Staatsbahn zusammen, um zu konsolidieren und das Schienennetz im Sinne des Gemeinwohls weiterzuentwickeln.

Ein Rückblick zum Ende des Werks zeigt, wo die Rheinische Eisenbahngesellschaft Spuren hinterlassen hat, welche Strecken bis heute im Betrieb sind und wirtschaftlich ein Rückgrat bilden, aber auch, wo bestenfalls Radwege alte Trassenführungen zeigen.

Gespickt mit historischen Aufnahmen und Zeichnungen sowie Fotografien aus der älteren und jüngeren Gegenwart liegt eine Lektüre vor uns, die nicht nur kurzweilig zu lesen ist, sondern viel Wissen vermittelt. Wer Interesse an der Geschichte der von der RE erschlossenen Regionen und der Eisenbahn im Speziellen hat, kommt hier nicht zu kurz.

Die Inhalte hinterlassen Eindrücke und auch Ideen. Wer die Historie der deutschen Eisenbahn versteht und sich in frühere Zeiten hineinversetzen kann, der wird seine Projekte auch umso authentischer umsetzen können. Deshalb ist dieser Band für Geschichts- und Eisenbahnfreunde wie auch Modellbahner gleichermaßen geeignet.

Publishing pages:
<https://www.verlagshaus24.com/sutton>

Die Kurzhauber mit Stern

Mercedes' Erfolgstyp

Kurzhauber blieben nur ein Übergang von den langen Schnauzen der Vorkriegszeit zu den modernen Frontlenkern. Dennoch währte ihre Bau- und Betriebszeit rund vierzig Jahre! Erfunden wurden sie keineswegs in Deutschland, aber hier waren sie auf lange Zeit geschätzt und beliebt – auch bei Modellbahnfreunden. Ein Buch setzt den Vertretern mit Stern ein umfassendes Denkmal.

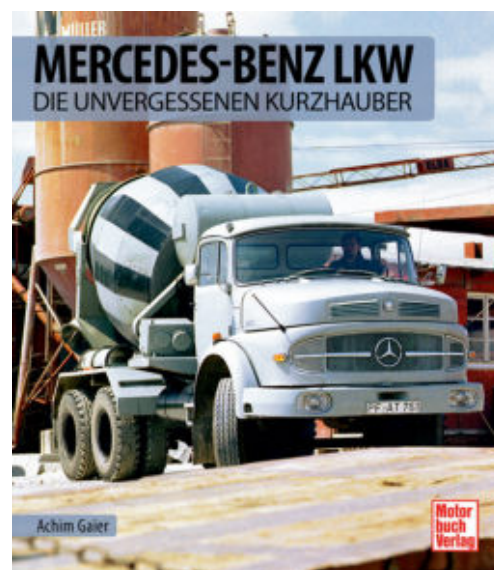
Achim Gaier
Mercedes-Benz LKW
Die unvergessenen Kurzhauber

Motorbuch Verlag
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Mit dem Zweiten Weltkrieg war die Ära der Langhauber-Lastwagen schnell vorbei. Alle deutschen Nutzfahrzeughersteller entwickelten frühzeitig neue Modelle, die statt der langen „Schnauzen“ nun deutlich kürzere Hauben besaßen. Für den Frontlenker schien die Zeit aber noch nicht reif, die neuen Typen markierten eine Übergangszeit.

Die Ergebnisse bekamen verschiedene Namen, mit denen sich die Marken auch voneinander absetzen wollten: Magirus hatte Rund- und Eckhauber, bei Mercedes-Benz wie auch MAN hieß die neue Generation Kurzhauber.

Doch welchen Namen wir dem Kind auch geben und wie das jeweils typische Gesicht auch aussah: Unverkennbar war eine neue Zeit angebrochen, zunächst geprägt von den runden Formen der Nierentischära.

Geblieben war nur, dass eine, wenn nun auch kürzere, Haube vor der Kabine dem Fahrer ein Gefühl von Sicherheit vermittelte. Und das sollte auch noch lange Zeit so bleiben. Im Falle von Mercedes-Benz endete die Produktion dieser Fahrzeuge erst in den neunziger Jahren!

Das vorliegende Buch ist in der Corona-Zeit erschienen, glücklicherweise aber noch ab Verlag erhältlich. Sein Erscheinen hatte sich, wie auch bei anderen Verlagen, durch die Folgen der Pandemie verzögert und kleinere Lücken erzwungen. Da wir es ursprünglich mit einem passenden Modellthema präsentieren wollten, stellen wir es Ihnen jetzt erst so spät vor.

Kurzhauber aus dem Hause Daimler-Benz haben schließlich auch im Maßstab 1:220 eine Tradition: Märklin legte solche Fahrzeuge für seinen Themenschwerpunkt „Das Werk“ auf, der vielen bekannte Zetties Michael Hering hat viele Fahrzeugvarianten im Eigenbau umgesetzt und mit Nano Models aus Italien hat aktuell ein Hersteller schwere Kurzhauber im Sortiment.

Entwicklungstechnisch folgten sie den Langhaubern, wie es bereits erwähnt wurde. Diese hat Achim Gaier bereits in einem Buch vorgestellt, das wir an dieser Stelle ebenfalls rezensiert haben. Der gebürtige Karlsruher und gelernte Buchhändler entdeckte seine Faszination für schwere Nutzfahrzeuge und ließ konsequent diesen Band folgen.

Wenn wir sagen, dass es genauso gut wie das vorausgegangene Buch gefällt, dann haben wir ein Fazit gleich vorweggenommen. Doch so einfach möchten wir es uns dann doch nicht machen. Die Inhalte seien hier noch hinreichend ausführlich vorgestellt.

Die Lastwagen der hier behandelten Bauform, ein Baukasten für verschiedenste Ausführungen und Typen, erschienen 1959. Der Motor befand sich nicht mehr vollständig vor dem Fahrerhaus, sondern war ein Stück in dasselbe hineingeschoben.

Ihr großer Erfolg rechtfertigt es, ihnen in enzyklopädischer Ausführlichkeit ein eigenes Buch zu widmen. Dieses liegt heute vor uns. Nachgezeichnet wird darin, warum der Kurzhauber entwickelt und 1959 eingeführt wurde.

Seine Technik wird ausführlich vorgestellt, ebenso die Werke, in denen die Fahrzeuge im Laufe der vielen Jahre gebaut wurden. Unterteilt nach Zeitabschnitten, werden die vielen Modelle und zwei Generationen strukturiert vorgestellt. Auch die neuen Typenbezeichnungen, auf die Mercedes-Benz in den sechziger Jahren umstellte, finden Erklärung – anderenfalls käme der Laie schnell durcheinander.

Die Spannungskurve findet ihren Höhenpunkt in einer ausufernden Typenvielfalt, bevor mit einem Kapitel zum langen Abschied der Bogen wieder abfällt. Blicke ins Ausland bis weit nach Übersee belegen, dass diese Fahrzeuge ein Erfolg und „Weltphänomen“ waren: Immerhin handelt es sich ja um die erfolgreichste Lkw-Baureihe von Mercedes-Benz.

Kein Weg führt an einem Anhang mit wichtigen Daten zu Produktionszahlen und den einzelnen Typen vorbei. Dieser basiert auf den Werken von Werner Oswald, die wir ebenfalls schon vorgestellt haben. In Summe füllt all das ganze 272 Seiten, eine durchaus beachtliche Menge im Vergleich zu anderen Fahrzeugportraits.

Dass diese auch verschlungen werden, ist dem sympathischen Schreibstil des Autors zu verdanken, aber auch einer reichhaltigen wie guten Bebilderung. Historische Aufnahmen, werbliche wie auch Werksdarstellungen wechseln sich ab mit Farbaufnahmen aus der jüngeren Betriebszeit und Diensten restaurierter Fahrzeuge – diese dann auch in passenden Kulissen.

Alles in allem geben wir eine klare Leseempfehlung für ein Buch, dass viele Hintergründe, die Entwicklung, Geschichte und Technik sowie Typen und Modelle der Kurzhauber bestens zusammenfasst.

Die vom Verlag im Vorwort genannten Lücken, die sich wegen der Einschränkungen der Pandemie nicht mehr schließen ließen und einer überarbeiteten Auflage vorbehalten bleiben, sind für den Leser nicht bewusst wahrzunehmen.

Und so werden sicher nicht nur wir uns an Lastern erfreuen, die in leichter und mittlerer Ausführung sogar noch bis 1996 gebaut wurden. Jedem von uns sollten sie also aus dem Straßenbild noch gut vertraut sein.

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<https://motorbuch-versand.de>

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Märklin 88808 - Diesellokomotive Baureihe 218

Vorbild: Dieselhydraulische Lokomotive 218 446-3 der Deutschen Bahn AG (DB AG). Achsfolge Bo'Bo'. Betriebszustand der Epoche VI in der Farbgebung ozeanblau/elfenbein.

Modell: Aufwendige, vorbildgerechte Farbgebung in der Ausführung der Epoche VI, mit Glockenankermotor. Mit vorbildgerecht vergrößerten Puffertellern. Beide Drehgestelle mit allen Achsen angetrieben. 3-Licht-Spitzenbeleuchtung mit warmweißen/roten LEDs mit der Fahrtrichtung wechselnd. Dunkel vernickelte Radkränze. Länge über Puffer 75 mm.

Highlights:

- Einmalige Sonderserie.
- Glockenankermotor.
- Aufwendige Farbgebung.
- Lok in diesem Zustand sowohl für Epoche VI, als auch für Epoche IV einsetzbar.

~~249,00€~~ 199,00€



Märklin 88251 - Akkutriebwagen BR 515 mit Steuerwagen BR 815

Vorbild: Akkutriebwagen BR 515 und Steuerwagen BR 815 der Deutschen Bundesbahn (DB) in ozeanblau/elfenbein Farbgebung der Epoche IV. Zustand um 1978.

Modell: Fahrwerke aus Metall, Aufbauten aus Kunststoff. Beide Drehgestelle am Triebwagen angetrieben durch einen Glockenankermotor. Fahrtrichtungsabhängig wechselndes LED-Spitzenlicht 3 x warmweiß/2 x rot. Hintere Spitzenbeleuchtung am Motorwagen abschaltbar. Nachbildung der Führerstände mit LED-Innenbeleuchtung. Angedeutete Inneneinrichtung mit Passagierinnenraumbeleuchtung. Sehr hoher Detaillierungsgrad mit feiner und aufwendiger Farbgebung und Beschriftung. Gute Zugkraft durch hohes Fahrzeuggewicht. Länge über Puffer ca. 217 mm..

Highlights:

- Glockenankermotor.
- Warmweiße/rote LED-Spitzenbeleuchtung im Wechsel der Fahrtrichtung.
- Hintere Spitzenbeleuchtung am Motorwagen abschaltbar.
- Fahrwerk aus Metall und Aufbau aus Kunststoff.
- Führerstandsbeleuchtung und Führerstandseinrichtung.
- Angedeutete Inneneinrichtung. Passagierinnenraumbeleuchtung

~~339,00€~~ 299,00€

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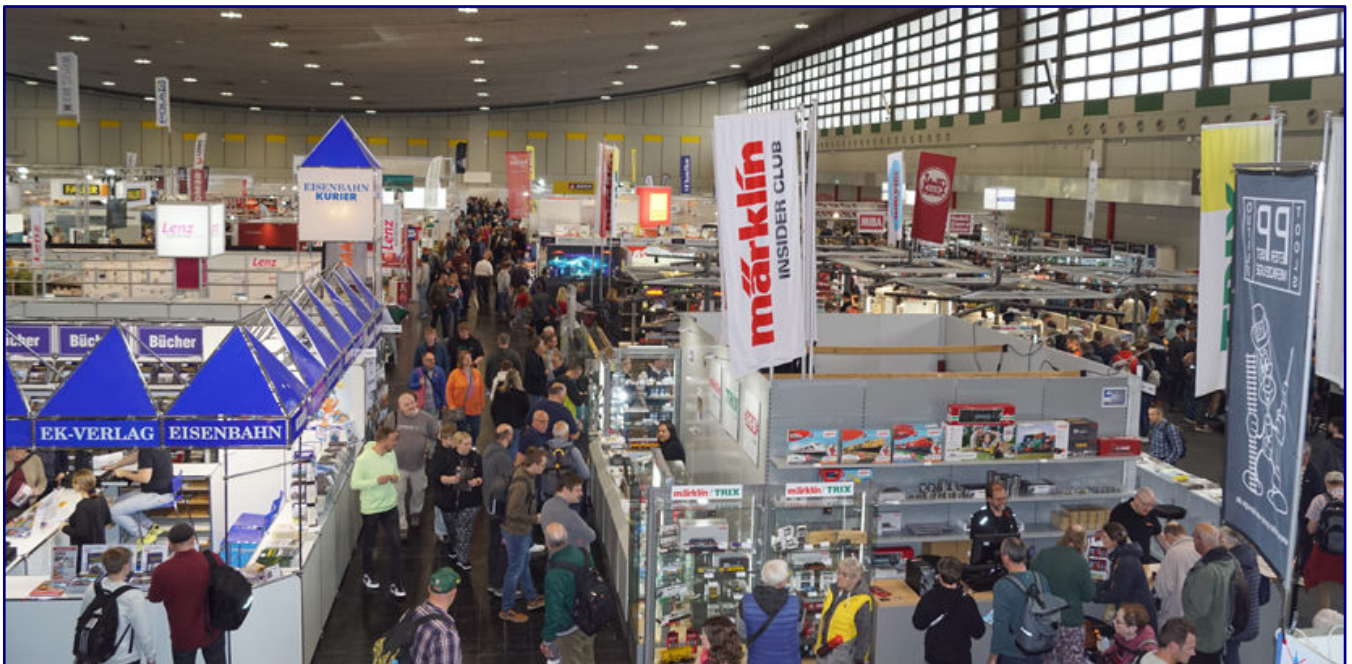
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How was Intermodellbau 2025 Under difficult Conditions

Intermodellbau took place again in Dortmund from 10 to 13 April 2025. With no Z gauge layout in sight beforehand, the inglorious departure of the organising model railway association brought good cheer and fresh impetus to the event. Apparently, nobody missed it. On the contrary: after sweeping up a heap of broken pieces, the trade fair took off to new heights.

Consistently full aisles and enthusiastic visitors: this is how we would like to summarise Intermodellbau 2025 in Dortmund in just a few words. And this was by no means to be expected in advance, because there were plenty of pitfalls to avoid.

This visible success is the result of a dedicated trade fair management, newly established contacts and networks as well as highly motivated model railway enthusiasts. Everyone pulled together and “fought” for a common goal.

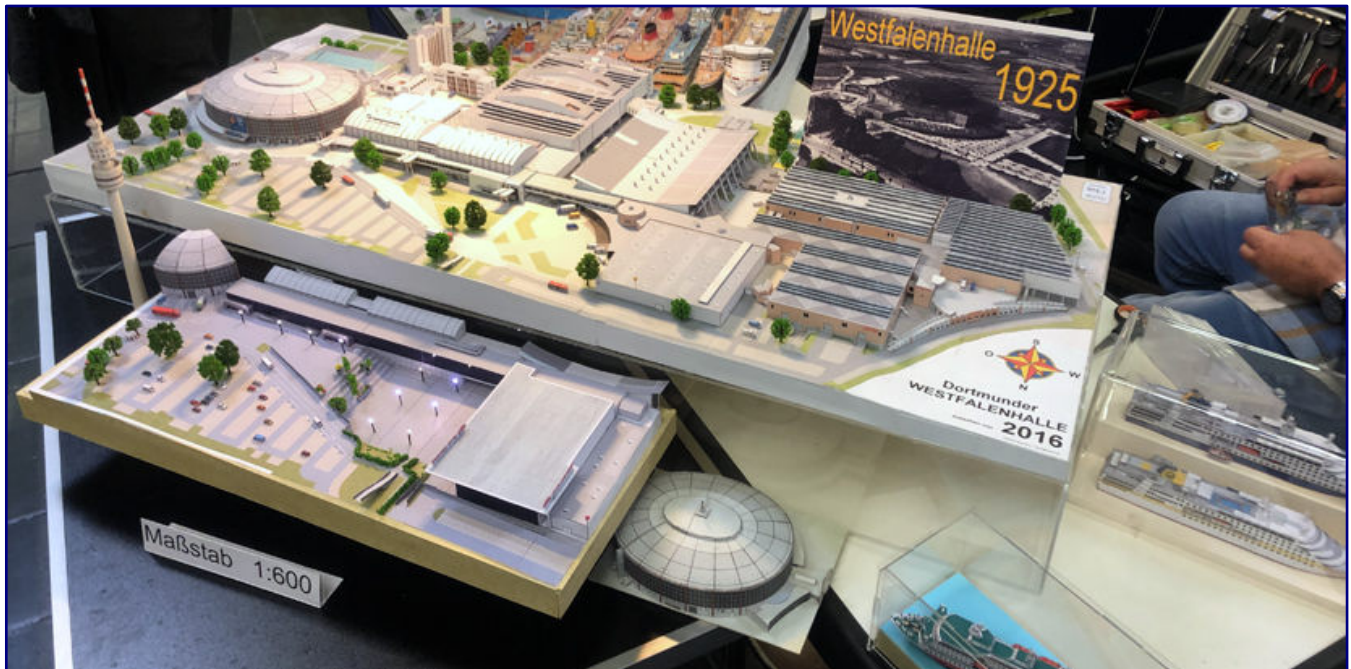


The halls were well filled on all days and at all times. We were not only able to fill a short-term gap in the model railway sector, but, in our view, we were even able to overcompensate for it in terms of quality.

For some time, it looked as if there was a big hole looming in the visitor magnet that is the model railway. At very short notice, but with all the more publicity, the previously organising model railway association cancelled a cooperation that had existed for over 30 years and cancelled all exhibitors in its area.

Messe Dortmund GmbH did not have much time to find new exhibitors or get back in touch with the cancelled participants in order to conclude new agreements. The fact that this worked out is largely thanks to the Bundesverband Deutscher Eisenbahn-Freunde e.V. (BDEF) and other associations and groups. As a media partner of the trade fair, we were also pleased to be involved.

As a result of this joint effort, there was no loss of quality and the number of exhibitors, with more than 30 systems were on display, actually increased. Everyone we spoke to personally was highly satisfied with the agreements reached regarding the costs covered by the organiser.



The organiser looked back on “100 years of Westfalenhallen”. A joint effort had made the model railway section of the anniversary a success after all. Kartonmodellbau celebrated with models of the site as it was in 2016 and the new exhibition centre (pictured in front) as it is today.

Some even told us that they had not been considered for this trade fair several times under the old “management”. There were also explicit statements that hotel accommodation and travelling expenses had previously been denied due to proximity to home. The reference point here is 1,000 km of travelling by car for a four-day trade fair participation plus set-up day, which should be covered privately!

This is probably why we did not meet anyone who was unhappy with the departure of the previous organiser – on the contrary, this was mostly welcomed as the right step into the future. The impressions we received on site do not match the statements of the previous partner.

On the other hand, the information that the trade fair was able to build on previous years with around 61,000 visitors, fits in with what we experienced ourselves. As mentioned at the beginning, the halls were remarkably full, and this was only to change shortly before the daily closing of the fair – much later than usual.

This also certainly speaks in favour of the quality of what was on offer from the 380 or so exhibitors from 17 countries. Railway modelling was traditionally to be found in Halls 4 and 7. There were also seminars and specialist lectures for model railway enthusiasts. In Hall 7, the BDEF offered its “Junior-College Europe”, where children and young people were able to try their hand at modelling.

Good response from the youngest

But there were also many other areas where the younger and youngest visitors could have fun. There were play areas for model railway manufacturers, where the combination of model railway and building



At many stands, visitors were able to tinker under expert guidance, such as here at the German Model Flying Association (DMFV).

blocks went down very well, as well as craft stands for small aeroplanes or a ship. Each modelling division wanted to give the children something to remember the visit by and to awaken a new passion.

They were allowed to ride on a lorry trailer with the RC model builders or obtain their own “small lorry driving licence.” Real steam unfolded for those who wanted to enjoy the experience of a park railway in Hall 8 and ride around the largest models on rails that this exhibition had to offer.

The programme also included a captain's patent, face painting for children, a painting competition (at EK-Verlag), Carrera track races and a wooden railway for the very youngest. At N-Club International, there was a search competition for four layout motifs, which we also take up in episode 39 of **Trainini TV**. So, there was certainly something for every age group and every taste.

The principle that the word “grasping” has more to do with children's hands than their eyes, now seems to be well established in their minds, which makes us very happy in view of the urgent need for new talent.

These impressions were also confirmed in many conversations with visitors. The hands-on activities were explicitly praised. The remote-controlled cars were particularly well received, be it for driving along on large representatives or for self-steering on smaller models.

Compared to previous trade fairs, we also received better feedback on accessibility: significantly more stands were also accessible for smaller people or wheelchair users, and this time we did not receive any indications that disabled parking spaces were too far away, although we also encountered very few wheelchair users.

It was praised that many exhibitors were prepared to take individual exhibits out of showcases and position them appropriately for wheelchair users. After all, this also shows that not everything can be optimally positioned for everyone and that it often only takes a little improvisation and good will to please everyone equally.

It was recognised in the best sense that the number of stands offering step stools for small children is increasing, so that they can also access a ride at eye level. Where this is not the case or is not feasible, this also led to corresponding comments: For example, one of the young visitors complained that he couldn't watch an RC car race because he couldn't see past taller people.



The BDEF once again offered its hands-on activities as "Junior College Europe." Children and young people diligently glued together kits and built small dioramas.

Traditionally, all modelling and model sports disciplines are represented in Dortmund. Compared to the previous year, the area reserved for building blocks has decreased, which we definitely welcome, as this is very far removed from classic model making and was also too strongly limited to a single manufacturer for our taste.

The robot show fights take up little space, attracting many visitors with their tools and fighting techniques, but also standing out very clearly from the rest of the modelling. The now important drone segment is also modest in this respect, but knows how to attract attention with spectacular races and demonstrations – all in all, a good mix.

Fascinating robot models

Plastic modelling extends into the realm of fantasy, which is an important segment because it also has a major technical influence on other sectors. This immediately reminds us of replicas of actors and objects from the War of the Stars series.

As a cinema fan of the classic trilogy, but also of the following two three-partners and two special films, the R2 units will certainly be remembered for a long time. Audience favourite R2D2 also impressed with the original sounds provided by Lucas Film Studios. But other robots were also on show, including “Wally”, a robot that does not belong to this film series. Some of this is captured in the aforementioned episode 39 of **Trainini TV**.

They had probably all come to congratulate the Westfalenhallen on its 100th anniversary. That’s how long ago the first Westfalenhalle was opened. This inspired model aircraft builders to create a 100-year Westfalenhallen plane, while the model ship builders were more interested in birthday cakes learning to swim.



The robot models from the “Star Wars” film series proved to be very popular. Young and old alike followed the movements of R2D2 and another R2 unit with great interest.

The cardboard modellers came up with a fitting idea: using their techniques, they depicted what the hall once looked like, and how the site appears today. Last year we filmed on the forecourt against the backdrop of the new exhibition centre, and now we experienced the successful reproduction of this area on a scale close to our own.

The SMC Oberhausen lighthouse exhibition was also well worth seeing. The variety of such navigation signs – no two are alike – is impressive and there are certainly many exhibits that are not only appealing for replicas, but have also been realised directly on a scale that suits us.

Another classic feature of the trade fair is the 500 square metre water basin, where a wide variety of ship models are presented throughout the day to cater for all visitor interests. It is correspondingly crowded there at any time of day.

Fairground modelling is also always very lively and was present in Halls 4 and 7. This is due to the powered rides, the effective lighting, and also the music and announcements that create a model-like atmosphere.

In contrast, police and fire brigade models are usually much more static, sometimes presented on layouts or dioramas. Only good connoisseurs will discover something new or the effort involved in constructing their own models, because in the end they are indistinguishable from standard models or kits.

The Dortmund fire brigade model construction group took a new approach: they quickly filled an open space with a historic baby emergency ambulance (Baby-NAW) and a current ambulance (RTW) from the Johanniter-Unfall-Hilfe in the eastern Ruhr region. And the “discussion” was already open, the interest of the visitors was aroused.



Many readers have already seen the (hopefully) well-known model by Torsten Schubert. At Intermodellbau, it met its large model for the first time, which was demonstrated several times a day.

Within sight at the Waltrop stand, visitors could marvel at the model of the Dortmund RTW, which Torsten Schubert illuminated based on a model. The author of this article recorded the complete lighting, including the special signalling system, as a video years ago.

With the help of electronics developed by Meyer-Modellbau, Torsten was then able to insert all the components of the system into the model and control them. For the first time since the model was built, the prototype and model met, so visitors were able to see for themselves that everything was correct and nothing had been left out.

Rescue service instructor Marc Glauer demonstrated the vehicle several times a day and answered many questions from the guests. They were interested to find out how the rescue service system works, what measures are taken in such a vehicle, and what operational tactics are used in major emergency situations.

Using the example of the Baby-NAW, which has long since had a successor, they were also able to see how the overall system has been improved and supplemented with donations from private individuals and companies in Dortmund, after all, babies and children are not small adults!

Model railway visitor magnet

The focus of the model railway and thus a traditional visitor magnet was once again to be found in Hall 4. Well-known and long-standing exhibitors with relevance for our scale could also be found there this year: Busch, Das Kantoor, Faller, Luetke Modellbahn, Noch, and Weinert Modellbau and Werkzeuge Peter Post.

Lantern specialist Beli-Beco, which regularly appears in our reports from Nuremberg, was represented in Dortmund for the first time. Zetties were finally able to get a personal impression of the quality that we always emphasise so much.

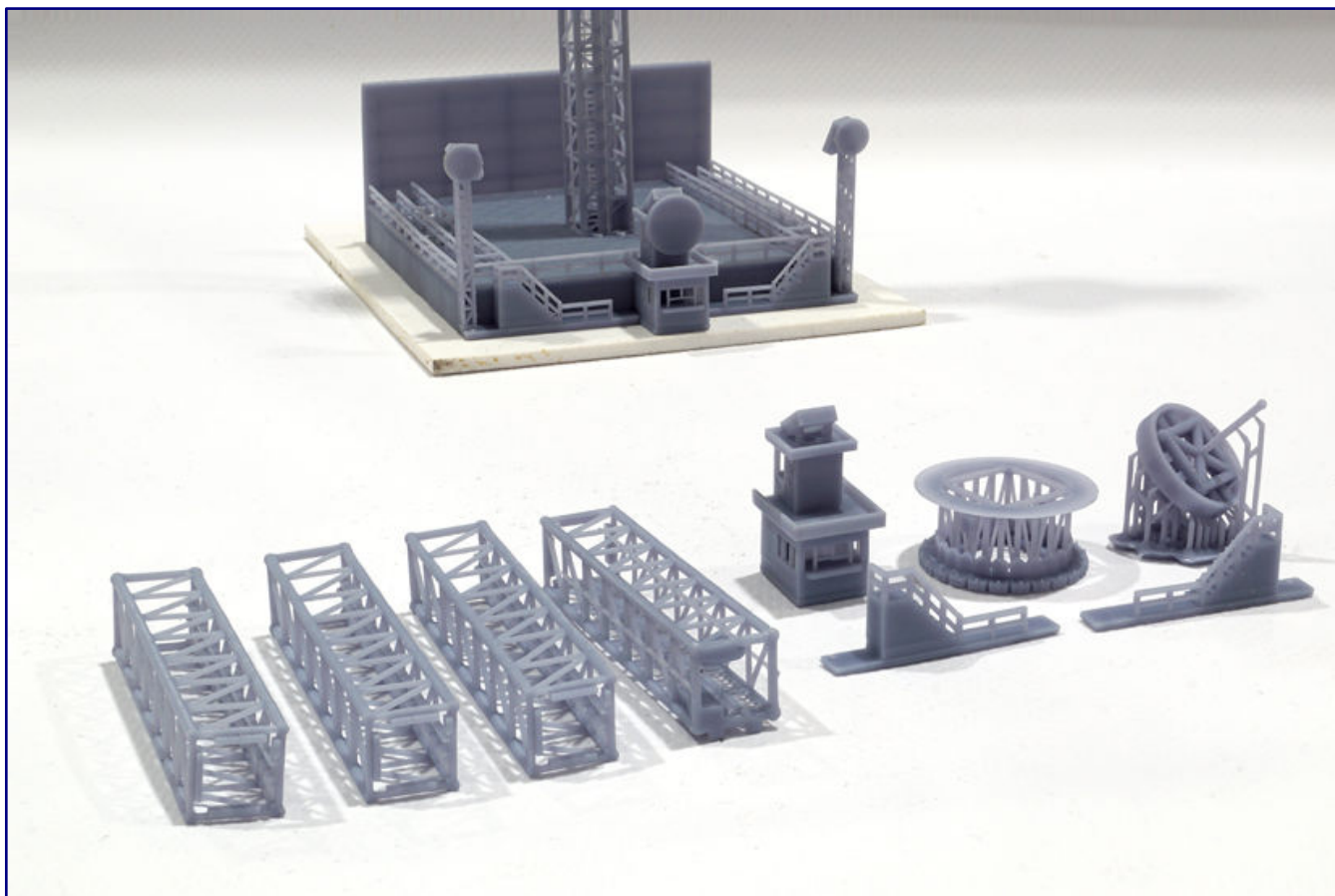
Artitec had freshly arrived new products with them and offered them for sale for the first time (see reports in the April issue). We already reported on this in last month's news. Uhlenbrock Elektronik exhibited the new Intellibox 3 in its showcases. All the guests' questions about the device, which is due to be released in June, were answered in detail.



The "Jura World" amusement ride is now also set to become an eye-catcher at the funfair as a model from Imprintium. Meanwhile, the dinosaur figures could certainly be used to provide a twinkle in the eye in other ways too.

Imprintium had announced in February that its new products would not be ready for demonstration and sale until this trade fair. At the time of our visit, a VW Bus T3 with a lifting roof for camping scenes and two locomobiles (steam tractors) for agricultural scenes in eras I and II were still available.

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The free-fall tower kit (photo above) is another spectacular innovation from Imprintium and will be impossible to miss at a funfair due to its height. The VW Bus T3 with extendible roof (photo below), which was once commonplace for us, is more modest.

The “Jura World” fairground ride is a special kind of eye-catcher. With matching life-size dinosaur figures, it will certainly not be overlooked and will not only be of great interest to children. Without the stand, they could also be used to enrich a forest. We’re not even thinking of a dinosaur park, but of scenes with a surrealistic character that will make onlookers smile.

The free-fall tower kit is impressive in size when it is assembled. It is then the visual centrepiece on the fairground and deserves effective lighting to emphasise the special atmosphere of such funfairs.

A special presentation took place at the Märklin stand on the first day of the fair. It was announced, at least in part, because the AC/DC train in H0 scale was to be unveiled there. However, the 500 visitors present were kept a little “on the edge of their seats”.



The Märklin stand was completely empty when Managing Director Wolfram Bächle presented two surprise innovations.

However, the speeches by Märklin Managing Director Wolfram Bächle, who was also available to answer questions afterwards, and Exhibition Director Sabine Loos initially focussed on a special train for gauge 1: the historic “Edelweiss” luxury train from CIWL.

Z gauge was also well represented on the stand. In addition to the exhibition layout, there were other exhibits in the display cases. Samples of the latest innovations were on display there, including for the first time test sprayings of the announced 1928 passenger coaches – raising expectations of an interesting family of coaches that will finally bring a breath of fresh air to the passenger transport segment.

There was also a small gift for the club members. Printed with a matching catalogue motif for LGB, Trix, or Märklin, this was a small “booklet” full of sticky notes and sticky flags: a valuable utensil for the haptic experience with books and other writing materials.

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Wolfram Bächle praised his employee Ulf Bovensmann (photo above; left) as an important source of ideas for the AC/DC steam locomotive (centre). Another surprise novelty was the CIWL luxury train “Edelweiss” with a French steam locomotive (photo below) as a counterpart to the DRG’s FF “Rheingold” at the time.

The specialised publishers are relevant across all gauges. Loki / Semaphor, Stiletto, VGB-Geramond and also EK-Medien were on site. In addition to its favourable book offers, the former EK publishing house also had colouring pages for the youngest visitors this year.

In addition to bargains from stock sales, Geramond also had the latest magazines and books on offer. On the evening of the first day of the fair, the editors of the trade magazines invited the model railway manufacturers to award the “Golden Track” at the BDEF stand. Märklin was also successful here and received the award for the 1:220 scale ETA / ESA 150 battery-powered railcar, among other things.

The railway magazine presented further sights at the same location with its diorama competition on the subject of “Lost Places.” Visitors were invited to vote and could also choose from three Z gauge entries, including a station re-use, an area with a forgotten steam locomotive and overgrown signal box as well as a Sleeping Beauty castle with a railway siding.



Of the three Z-gauge entries in the EM diorama building competition, this one stood out in terms of its depiction of wild nature.

Within sight, clearly visible from several sides and connected to other modules, was the N-gauge layout “Cinema City” by Markus Schiavo (GermanNTrak). This showpiece with urban US motifs scenes from music, films and series and presents them in such a way that the viewer can go on a discovery tour for hours.

Strong Z-gauge presence

Under Moba's direction, Z gauge remained only a side note at the trade fair and was at best represented with a diorama-sized showpiece alongside the Märklin display. The change in the organisation obviously also did our scale some good, as a visit to the fair was now definitely worthwhile for Zetties.

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The N-gauge layout "Cinema City" (photo above), which was presented by Markus Schiavo, was also popular with visitors. In the streets of the city, we find many film scenes and other celebrities. The easiest place to find them is certainly in the "Cinewood Museum" in the front part of the layout, where we couldn't miss this extremely well-known spaceship scene.

Hopefully, RTS was also well received, as the new Brandon speed controller was also on display at the stand of the greenkeeper supplier and controlled a train on a circular layout.

However, this innovation, which is based on an invention by Jeremy Brandon, is not yet available.

The kits from MBZ Modellbahnzubehör and Modellbau Laffont were also on sale, as both Thomas Oswald and Stefan Laffont had found their way to Dortmund and presented their programme to customers there.

Michael Boost had his suitcase system in his luggage. His digitised models made their rounds on it, including the Wismar rail bus from Märklin, the most recent delivery that he has worked on.



How much Jeremy Brandon is in the new Brandon speed controller? RTS demonstrated a sample at its stand.

He was also able to present the latest version of his digital lighting board for passenger carriages, which visitors were able to see for themselves. Co-exhibitor Dietmar Allekotte had this time focussed on the H0f gauge and, thus, on 6.5 mm track, but on a different scale of 1:87.



In the real steam section, we found this gigantic model of the Norfolk & Western Railway's "Class J" 611 — certainly not a showpiece for the living room at home.

The Z-Car system was also represented, with new chassis and a future development stage. This will also include automatic vehicle stops at red lights. Developer Oliver Kessler was also there in person on the first day of the fair to answer questions from customers and interested parties.



Michael Boost demonstrated his newly developed DCC lighting board, which can be divided as required from 35 mm (photo above). Stand colleague Dietmar Allekotte focussed this time on H0e gauge light railways (photo below).

The largest stand for our gauge in terms of space was that of Birgit Foken-Brock (Trafofuchs), where **Trainini®** was a co-exhibitor. Together with Peter Burgard, she had set up a large modular layout that had never been seen before at Intermodellbau.

The film shows many scenes that she has created with her self-designed characters. On **Trainini TV**, she explains some of her personal favourite scenes in episode 39. The two showcases on the stand also displayed a selection of the figures and car models, all of which have long since been designed using a CAD programme and output from the 3D printer.



In the Rhine-Ruhr station on Birgit Foken-Brock's modular layout, the ETA 150 and its ESA 150 driving trailer belonged to the same station as in the Ruhr area. A familiar scene.

She painted the blanks she had brought with her in front of the audience and, when asked by the astonished audience, also gave information about the tricks she uses to achieve such intricate details.

The rolling stock on the exhibition layout was a mixture of current models from the Azar Models and Märklin product ranges and classics in our nominal scale. The newly interpreted concept of banana wagons was also to be found here.

Ralf Junius and Holger Späing helped to fill the two display cabinets on behalf of the editorial team. In the smaller glass cabinet, which looked like a table with a glass surface, there were several small dioramas on display. The large display case was filled with a variety of items. Three end carriages of Shinkansen trains from the Tomytec programme for N gauge were also on display there – a look outside the box.

The majority of what was shown was, of course, Z scale. The airport control tower, which we presented as the first paper kit in this magazine, was on the lowest



The showcases included the E91⁰ series from Noba-Modelle and the NATO wagon from Märklin.

level. Two Eurowings aeroplanes, including the Borussia Dortmund 'Team Airbus', were positioned in front of it to make it look prototypical. It provided motivation for the match against Bayern Munich, which took place on the weekend of the trade fair and resulted in a draw.



The concept of the banana wagon has been reinterpreted. The deceptively real-looking mini fruits certainly attracted a lot of attention.

The diorama "Haltepunkt Grund 2.0", which we created especially for the Märklin battery-powered railcar, was presented to a large audience for the first time. This train was also presented on the showpiece in an ocean blue and ivory colour scheme.

With the class 191 from NoBa-Modelle, we referred to a recent article and gave the stand guests the chance to get their own impression of this locomotive, which had been the subject of controversial discussion.

With both versions of the DB Kübelwagen, we were also able to show samples of spring new products that had not yet been seen anywhere else. And the NATO advertising wagon from Märklin proved that it is not an April Fool's joke and actually exists.

The therapy diorama "Am Haarstrang", which is also equipped with Trafofuchs figures, was designed to extend the time spent at the display cases. This was quickly converted into a search game: Visitors had to find the scaled-down slow worm, which was sunbathing on it. The second question was how many frogs were hiding in the water lily area.

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Photo above:

A large cemetery can also be seen on the modular layout. Our figure artist has also immortalised herself there. She visits her mother's grave and waters the flowers there.

Photo below:

How many lake frogs are hidden in the water lily field of the "Am Haarstrang" therapy diorama? There are two, but who will actually find them? The male Gisbert in particular doesn't make it easy for viewers.



Looking back towards the Märklin stand: In the new product display cases, sprayed samples of the new 1928 - 1930 model passenger coaches were shown (photo above), which included all three basic coach designs. Club members received a sticky note booklet in the form of a current Märklin catalogue as a trade fair gift.

The correct answer was “two” and the special thing about Gisbert, the smaller of the two amphibians, was that, contrary to the real-life model on the diorama, he has a girlfriend and the sight of the lady frog knocked him out. Who could have guessed that? This provided plenty of opportunity for nice conversations and creative ideas, which many people took home with them.



We also found what we were looking for among the cardboard modellers when it came to railway models. However, the battery-powered railcar type Wittfeld and the DB conversion wagon are not on a scale of 1:220.

The best for last

As already mentioned at the beginning, this year's Intermodellbau impressed with a freshness and quality of layouts that seemed both unexpected and new in its entirety. The smooth departure of Moba has brought a breath of fresh air. It seems impossible for us to honour all the layouts on display here.

We have selected three of them for the H0 scale that couldn't be more different: For example, we were impressed by “Bouy-à-Bèze” from the Het Spoor modelling club in Belgium. Its coastal motifs were lovingly realised down to the smallest detail and held our attention for a long time.

This also applied to Wysoka Gorzowska in Poland, where we were captivated by the scenic green backdrop of the branch line atmosphere. Not even a PKP armoured train could break up this idyllic scene.

A German première was the installation presented by MBF Maifeld with Ruhr area motifs from the era of the coal and steel industry. Train traffic breathed life into the scene and, in addition to heavy coal and steel trains, also reminded visitors of how this metropolis was connected to the Bundesbahn's F-train network.

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Two absolute highlights for us among the exhibits on show were "Bouy-à-Bèze" from the Het Spoor modelling club in Belgium (photo above) and Wysoka Gorzowska from Poland (photo below).

No matter how exhausting four days of trade fair in a row may be: when the clock struck 5:00 pm on



The Wismar rail bus from Märklin made its rounds at MBZ Modellbahnzubehör. It had the boxes and cases from Trafofuchs on the roof, which we already mentioned in the test report.

Sunday, we were exhausted, but, somehow, also disappointed that we would now have to wait another full year.

We can only advise all interested parties to enjoy the breath of fresh air in Dortmund and to make a note of 16 to 19 April 2026 in their diaries. That's when the next big event for experienced modellers as well as newcomers and families will take place at Messe Dortmund!



Exhibitor selection with Z gauge relevance:

<https://artitec.nl>
<https://beli-beco.de>
<https://www.busch-model.com>
<https://www.das-kantoor.de>

<https://donau-elektronik.de>
<https://www.eisenbahn-kurier.de>
<https://www.faller.de>
<https://www.geramond.de>

<https://www.herpa.de>
<https://imprintium.de>
<https://www.luetke-modellbahn.de>
<https://www.maerklin.de>

 MBZ
<https://michas-moba.de>
<https://modellbau-laffont.com>
<https://www.noch.de>

<http://www.peter-post-werkzeuge.de>
<https://www.rokuhan.de>
<https://tams-online.de>
<http://www.trafofuchs.de>

<https://viessmann-modell.com>
<https://www.vgbahn.shop>
<https://weinert-modellbau.de>
<https://z-car-system.de>

Trainini TV – Folge 39:

<https://www.youtube.com/TraininiTV>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Praise for the cover story of the last issue:

A great cover story in the magazine that inspires me. Incidentally, the 155 as the basis is by Michael Pfeiffer, the man who also designed the ZFI logo, but am I the only one who's triggered by the fact that the lettering "DB Cargo" is simply shifted to the right in this absolutely ingenious and fantastic realisation of the 155?

The "a" from the lettering should be roughly centred on the locomotive – here Alex has placed the DB logo slightly to the right of the centre and the cargo is somewhere in the landscape. That looks very unusual to me. There were definitely 155s with off-centre logos (DB and also RAILION DB Logistics), but I'm not aware of any such DB Cargo variant.



Torsten Scheithauer provided important information on the origin of the model and also the correct positioning of the owner's markings: Apparently, Roco had once made a mistake here, which Alex Mark has probably also adopted.

Could this be due to the fact that Roco has also realised this in a staggered manner on its H0 model 62623? (...) But the prototype was rather "normally" labelled. (...) Interestingly, Tillig also took on the 155 260-3 in TT (article 04332) (where there were so many prototypes), unlike Roco also with normal lettering in the centre. (...)

And the photos show the same locomotive number from the same side. So please don't get me wrong: I was very pleased with the report in Trainini as a theme, and Alex has designed and rebuilt the locomotive

wonderfully, with lots of great details, but when it came to the lettering, he was probably misled by the faulty Roco locomotive? At least I can't think of any other reason for this "labelling error."

Torsten Scheithauer, Düsseldorf

Editor's reply: We have spoken to the builder of the model and the following description has emerged. The housing was completely empty in the roof area and was completely fitted by him. Alex Mark can no longer remember the model for the chosen locomotive number, but the assumption about the Roco locomotive, which could have been used as a guide, is probably correct.

There are also a few errors in our article. For example, the handle bar under the cab front wall is an etched part that also includes the lamp ring. This is why the lower lanterns were also given such rings. We also made a mistake with the lettering: this was his first model, which he labelled using pad printing technology. The decoder is not from ESU either, but a Velmo LDS223331.

Dissatisfaction with a kit:

I always enjoy reading your magazine, as I've been dabbling in the zettie world for almost a year now... I'm building a 70 x 40 cm railway depot diorama that will fit on a cupboard shelf (maybe I'll send some photos?)

So far, I have almost only used Archistories kits for this, which have always fitted perfectly — unfortunately, not with the coal store. There was a lot of sloppiness, the floor is crooked, some of the tenon joints don't fit into the corresponding slots, so I had to re-file several times... I suspect manufacturing pressure because the parts took so long to arrive?

This is a brief experience report...

Bernd Moritz, by E-Mail

Editor's reply: Not only at Archistories does every kit undergo multiple trial assemblies until everything fits as planned and a new product is then ready for production. As the photos for the packaging have been produced by one of our editorial colleagues for several years, at least one kit has also been assembled there.

It can be confirmed that in this case some parts have to be installed outside the familiar routines, e.g., the suspended ceiling in the main house is glued in without mortising, but at the same time with two façade parts it holds immediately. Incidentally, Archistories has accommodated the customer here with the window foils – they are custom-fit parts. Previously, there was only the window arch.

This has certainly not only led to unfamiliar handling for you and harbours a potential for error that cannot be completely ruled out. Such feedback is therefore always valuable for the producer in order to critically revise the accompanying instructions and make changes where necessary. If a faulty component is found in the kit, in this case obviously a crooked one, please contact the dealer, who will work with the manufacturer to find a replacement.

Reference to own Dinamo experiences:

I once posted a very detailed report on Dinamo with iTrain for Z gauge in the ZFI forum: <https://f.z-freunde-international.de/viewtopic.php?t=17912&start=10>

Klaus Steuer, by E-Mail

Design proposal for Artitec:

Thank you for the new Trainini booklet. I was also immediately impressed by the Artitec new product "Furniture lift 322.041".

Bought directly and put together a scene on "Werdersheim" with it: "The furniture van is coming." I had already converted the green Büssing lorry "Samentransport" into a furniture van with a trailer, but the combination seemed too heavy for the streets of Werdersheim.





Our reader has attached the furniture lift to his converted Büssing lorry. The main town on his estate will soon have new residents. Photo: Dirk Rohwerder

With the new furniture lift as a trailer, it looks airier and more credible. A new family from Hamm will soon be moving into Werdersheim.

Dirk Rohwerder, Sprockhövel



219 001-5 also took part in the celebrations to mark 25 years of the "Moorexpress". The former gas turbine locomotive has been operational again for several years and is active in goods train service. Photo: Jörg Endreß

Inspiration from Moorexpress:

Yesterday I had the opportunity to take part in the celebrations to mark the “25th anniversary of the Moorexpress”. The star of the exhibition was 219 001, the centrepiece of the NDR cult series “Roland and his locomotive”. There will be another instalment, and a film crew was also present yesterday. I had the opportunity to visit the driver’s cab and engine room.

What I wasn’t really aware of: Gnarrenburg is a centre of the glass industry and this was one of the motivations for building the BOE route. The ground floor of Gnarrenburg houses a small museum on this subject.



The EVB's VT 151, a class 628⁴ diesel multiple unit, which was christened for the festival, is wearing extremely attractive colours.
Photo: Jörg Endreß

Among other things, it contains a diorama of a glass factory; a good idea for a model railway connection. The newly painted VT 151, which was to be christened in the afternoon, travelled on a special timetable between Gnarrenburg and Osterholz-Scharmbeck.

I have uploaded my photos to my cloud and am happy to make them available if you have any editorial use for them. A prototype locomotive that has been in use for decades and in various (private railway) liveries should actually be a real find for the model railway industry. And the VT 628 could possibly also serve as a model for Märklin one day.

Jörg Endreß, Bremen

Märklin deliveries in May:

Märklin deliveries picked up speed again in May: A whole series of new models arrived at the dealers. The FD “Rheingold” opens the roundelay as a one-off series. We presented the three items with which it



The private Kühltransit A.G Hamburg insulated car (item no. 86620) has Era III lettering and was used on the German Federal Railroad.

is modelled in detail in the article at the beginning of this issue. This also applies to the three-domed version of the 038 series with tub tender (item no. 88909).

The Kühltransit A.G. (86620) insulated car with a brakeman's cab based on the association design, whose prototype was used by the German Federal Railways, also arrived at the dealers as a new item. This model is labelled for Era III and also in fine form.

Latest new products from AZL:

The first delivery for May concerns the GE ES44AC of the BNSF with the so-called Swoosh logo (item no. 62402-4 / -5). The next two locomotives have EMD prototypes: F7A of Jersey Central Lines (62919-1 / -2) and SD50 of KCS (65007-1 / -2).



F7A of Jersey Central Lines (item no. 62919-1; photo left) and ALCO RS-3 of Nickel Plate Road (63325-1; photo right). Photos: AZL / Ztrack

The SD40T-2 tunnel locomotive, also built by EMD, was given three Union Pacific road numbers (64103-1 to -3). This segment is completed by the ALCO RS-3 for Nickel Plate (63325-1 / -2) in Phase III design.



SD50 from KCS (65007-1; photo left) and Caltrain freight car (92022-1; photo right). Photos: AZL / Ztrack

The modernised 1937 AAR coaches of covered design with a prototype length of 40 feet are now available as CB&Q vehicles. Single (905401-1) and double packs (915401-1) are available. The freight car with side lookouts is currently travelling for Caltrain (92022-1 / -2).

Covered Trinity NSC coaches are available for the TTX with the new logo individually (905601-1), double (915601-1) and in packs of four (915626-1). The modernised Trinity tank wagons, on the other hand, are only available for GATX as single (905512-1) or double (915512-1) units.

Two 4180 BNSF pneumatic unloading cars (916051-1) bring up the rear of the freight car offerings. These buffer cars are actually lined up behind tank cars in front of a push-pull locomotive as a safety precaution at the end of the train. The latest new items are 53-foot containers for XPO Logistics (95114) and CSX (95107 / 95112).

Military new products at Panzer-Shop NL:

Panzer-Shop NL (<https://www.panzer-shop.nl>) has new military models in its range. The heavy transporters Faun SLT-50 Elefant (50 tonnes), Faun SLT-50 Elefant (56 tonnes), and Faun SLT-56 Franziska for Leopard transports come from the stocks of the German Armed Forces.

The BTR-70 was a Soviet-designed armoured personnel carrier that was used by the NVA in the GDR as the SPW-70. The IFA W50 LAK2 (with box) and the Trabant 601 as a Kübelwagen (bucket truck), with and without a tarpaulin, also belong to this category.

For the last few days of May, this provider is also offering double Armour Points. Until then, ten instead of just five of these bonus points will be awarded for every euro of purchase value. They can be redeemed for rewards such as discounts or free products.

Store bulk goods with Yellow Dwarf:

New this month at Yellow Dwarf (<https://www.yellowdwarf.eu>) is a bulk goods store (item no. 60319) for coal, gravel, soil, etc. This can be used to set up



The bulk goods store (item no. 60319) can hold coal, gravel or soil. Photo: Yellow Dwarf

a small depot, a coal yard or the yard of a speciality or construction company.

Anniversary of the Spielwarenmesse:

Spielwarenmesse eG is celebrating its platinum anniversary this year, as it was founded in Nürnberg (Nuremberg) in 1950. The following year, it organised the first Spielwarenmesse (toy trade fair), which means that the 75th anniversary will also be celebrated there at the beginning of 2026.

In 1949, four entrepreneurs decided to found the Spielwarenmesse. After the successful premiere of the first German toy trade fair, the cooperative, still an “exotic” company form for trade fair organisers today, was sealed in 1950.



The organiser celebrated its 75th anniversary in 2025, with the Spielwarenmesse as its biggest event next year.

The big birthday party will then take place in Nuremberg during the next trade fair from 27 to 31 January 2026. “We will celebrate this special event with all participants – it is a moment of appreciation for what we have achieved and an incentive to carry our success story into the future together with the industry,” says Christian Ulrich, Spokesman of the Executive Board.

Atlas tracks at Case Hobbies:

Peter Scheele from Case Hobbies (<https://case-hobbies.de>) reports that Atlas tracks are available again. Stock is therefore currently available. However, there will be no further deliveries from China in the foreseeable future.

This is probably due to the questionable tariff policy of the United States of America, which was also explicitly mentioned by the retailer in his message. We are already seeing the effects with other manufacturers as well.

Summer new items published:

Märklin announced its 2025 summer innovations on 15 May, unusually late, as the Intermodellbau Dortmund was usually used for this. But the long wait was well worth it, especially for the Zetties.

The Flixtrain appears as an MHI one-off series in the current version for Era VI. It is pulled by a new version of the Vectron class 193 with now two single-arm pantographs on the roof (item no. 88239). The train is made up of two car sets with three passenger coaches (87403), consisting of Bmmz 264.4F and 264.4, as well as two passenger coaches (87404) Bmmdz 268.7F and 264.4F.

This is probably the last time that a class 101 express locomotive (88668) will have a Märklin advertising design, because this class is already being retired. The prototype, which has not yet been painted, commemorates the 100th anniversary of the class 01 with the steam locomotive running gear shown. It is also an MHI special edition.



AZL is having the Swiss RAe TEE II multiple unit train (item no. 88730) made for Märklin in a six-car version for Era IV. Photo: Märklin

The Kittel steam railcar (88147) is returning as a regular catalogue model. The vehicle designated as DRG Cit Wü 05 now has the green paint scheme of the Reichsbahn passenger cars. The series of museum locomotives continues with V 80 002 (88801), which bears the "Deutsche Bundesbahn" lettering.

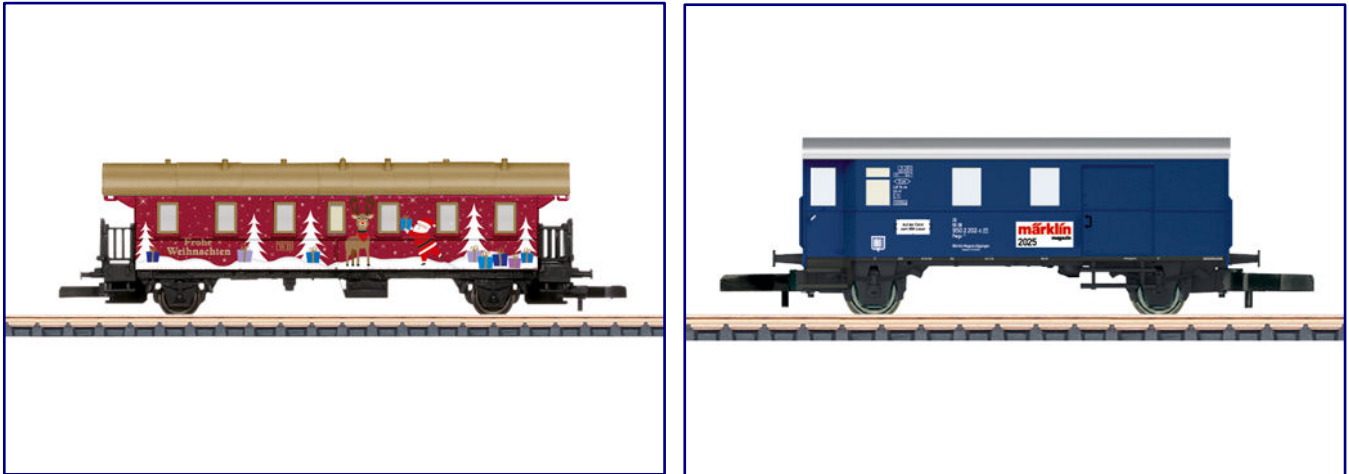


V 80 002 (88801) continues the series of museum locomotives and is, therefore, supplied in a wooden box. Photo: Märklin

The prototype that was once destroyed in the fire in Nürnberg (Nuremberg) had white lines on the lower edge of the frame in the area of the bogies, in contrast to the operating condition at the time of delivery.

The template for this incorrect design was a retouched photo of the manufacturer MaK. The model is technically reworked at the cable feeds of the bogies to ensure better curve running.

One colour variant is the service tank wagon with brakeman's cab (82303), which was used to transport internal combustion fuels on the railway. The Christmas car (80635), a type Ci through car, and the Märklin annual magazine car (80835), a Pwgs 041 goods train baggage car painted blue, were expected new items.



The Christmas car (80635; photo left) and the Märklin magazine car (80835; photo right) were expected new items. Photos: Märklin

However, with the RAe TEE II express railcar (88730) in a six-car version for Era IV, the brochure surprisingly also contains a new design. The completely new development has already been shown at the exhibition of the Stammtisch Untereschbach e.V. in Cologne and will be supplied by AZL. This means that the multiple unit also follows their design principles, which include traction tyres, a drive in the motor car without electrical connections to the other cars, important for all analogue railroaders with unpowered stopping sections in front of signals.

Commitment to the SZL strategy:

Rob Albritton, one of the two AZL owners, commented on the new RAe TEE II and the strategy for the SZL (Swiss Z Lines) brand in the in-house forum. He explained that he had wanted to bring back this second brand for a very long time.

The last attempt was the cancelled SBB Ae 8/14 project, which was to be produced as a brass model. Now they have arrived at the RAe TEE II, which is being produced using plastic injection moulding and, in addition to a smooth exterior, i.e., the wagon transitions, should also have an operating speed very close to the prototype.

They are happy to have won the Märklin company for marketing and sales, for which the red and beige version is supplied as an OEM model. The subsequent painting as a "grey mouse" is to follow under their own responsibility via a new dealer in Switzerland.

It is part of the strategy to diversify via European models. However, AZL's US segment should not be neglected. However, the renewed focus on Europe is also clearly stated as a requirement in order to be able to compensate for slumps in sales as a result of US tariff policy and to be resistant to such developments.

The statement emphasises that Azar Models pursues a good model policy with French models. As not all of its own models are modelled on Swiss models, SZL is now looking at British models.

Quick decision at WDW Full Throttle:

WDW Full Throttle (<http://www.wdwfullthrottle.com>) also fears the consequences of the US customs policy and relates the decision to produce a new limited-edition model to this: "I've wanted to build this model for years! After putting off building it for way too long and seeing the impact of our supply chain/customs situation, I thought I better get it done while I still have wheelsets to finish it."



The two bulk goods wagons were permanently coupled in the prototype, shared a road number and counted as just one wagon for operational purposes. This is why the Southern lettering also runs across both car bodies. Photo: WDW Full Throttle

The new product is scheduled for May 2026! For the time being, it remains unclear whether the year mentioned (twice in the information sent to us) is a typographical error or whether the announcement is actually being made so early due to the circumstances.

The pack in question (item no. FT-2062) contains a pair of Southern Railway wagons permanently coupled together in prototype with the road number 102079. We would like to provide some more information about the prototype.

Connecting the south to the electricity grid massively increased the utilities' demand for coal. Before they had access to a large fleet of newly designed 100-tonne hopper wagons, the ever-innovative Southern developed a makeshift.

From its considerable stock of 50-tonne wagons, it coupled two wagons at a time with a drawbar so that there were handbrake wheels at both ends. These units were now treated as one wagon for tariff purposes and were only given one common operating number.

On the other hand, this reduced the transport rates and this practice was to last until the mid-seventies, giving the old wagons a "second life". Full Throttle is now taking up this special era of coal transport with this new product.

Herpa summer aeroplane 2025:

Herpa announces the following military new products for the summer in its current brochure for use in Europe in its Wings programme:

Panavia Tornado IDS of the German Luftwaffe (Art.-Nr. 573580),
Lockheed Martin F35A Lightning II of the Royal Danish Air Force (573603),
Ilyushin IL-76 of the Ukrainian Air Force (573610),
Antonov AN-72 of the Ukrainian National Guard (573627) and
Airbus A400M of the Belgian air component (573665).



The Ilyushin IL-76 of the Ukrainian Air Force (item no. 573610) appears in the Wings product range on a scale of 1:200. Photo: Herpa

In the civil sector there are:

Ilyushin IL-18 of Deutsche Lufthansa in the DDR (573689) and
Boeing 707-320 of Pan Am (573696).

The Snapfit range is enhanced by the following models:

Airbus Industries Beluga XL (613286-001),
Airbus A220-300 of Air Baltic (614559), and
Airbus A220-300 of Croatia Airlines (614566).



The Airbus Industries Beluga XL (613286-001; photo left) and the Airbus A220-300 of Air Baltic (614559; photo right) enhance the Snapfit models. Photos: Herpa

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